



PARTIN SETTLEMENT ROAD WIDENING ALTERNATIVES TECHNICAL REPORT

Submitted to:
Osceola County Transportation and Transit Department





Partin Settlement Road Improvement Project Alternatives Technical Report

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Executive Director

This report contains engineering information that fulfills the purpose and need for the Partin Settlement Road Improvement Project from Neptune Road to East Lakeshore Boulevard in Osceola County, Florida. The undersigned acknowledge that the procedures and references used to develop the results contained in this report are standard to the professional practice of transportation engineering as applied through professional judgment and experience. The preferred alternative in the report also is consistent with the preferred alternative approved by the Osceola County Board of County Commissioners. The undersigned therefore certify this report, and approve the evaluation, findings, opinions, conclusions and technical advice prepared for this project.

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TABLE OF CONTENTS

I. INTRODUCTION	1
II. TRAFFIC	4
III. PURPOSE AND NEED	9
IV. INITIAL ALTERNATIVES.....	10
V. ALTERNATIVES EVALUATION	27
VI. RESULTS OF THE PUBLIC INVOLVEMENT ACTIVITIES.....	47
VII. RECOMMENDED PREFERRED ALTERNATIVE.....	50

TABLE OF FIGURES

FIGURE 1: LOCATION MAP.....	1
FIGURE 2: EXISTING YEAR 2020 AM AND PM LEVEL OF SERVICE.....	4
FIGURE 3: DESIGN YEAR 2045 NO BUILD AM AND PM LEVEL OF SERVICE.....	6
FIGURE 4: CRASH HISTORY AND ASSOCIATED LIGHTING CONDITIONS.....	8
FIGURE 5: CRASH LOCATIONS.....	8
FIGURE 6: AADT AND NO-BUILD/BUILD LEVEL OF SERVICE.....	9
FIGURE 7: TYPICAL SECTION OPTION 1A	16
FIGURE 8: TYPICAL SECTION OPTION 1B	17
FIGURE 9: TYPICAL SECTION OPTION 2	17
FIGURE 10: TYPICAL SECTION OPTION 3	18
FIGURE 11: ALIGNMENT ALTERNATIVES HOLDING NORTH RIGHT-OF-WAY LINE – WESTERN PROJECT LIMITS.....	21
FIGURE 12: ALIGNMENT ALTERNATIVES HOLDING NORTH RIGHT-OF-WAY LINE - EASTERN PROJECT LIMITS.....	21
FIGURE 13: ALIGNMENT ALTERNATIVES HOLDING NORTH RIGHT-OF-WAY LINE - EASTERN PROJECT LIMITS.....	23
FIGURE 14: ALIGNMENT ALTERNATIVES HOLDING NORTH RIGHT-OF-WAY LINE - EASTERN PROJECT LIMITS.....	23
FIGURE 15: PEDESTRIAN BRIDGE.....	26





FIGURE 16: DRAINAGE MAP (BASIN 1 TO BASIN 3A).....35

FIGURE 17: EXISTING STORMWATER MANAGEMENT POND.....39

FIGURE 18: DRAINAGE MAP (BASIN 3A CONT. TO BASIN 7).....40

FIGURE 19: DRAINAGE MAP (BASIN 7 CONT. TO BASIN 8B).....43

TABLE OF TABLES

TABLE 1: EXISTING SPEED LIMITS.....2

TABLE 2: EXISTING YEAR (2020) AM AND PM PEAK HOUR INTERSECTION LOS ANALYSIS SUMMARY.....5

TABLE 3: EXISTING YEAR (2020) AM AND PM PEAK HOUR ROADWAY SEGMENT LOS ANALYSIS SUMMARY.....5

TABLE 4: DESIGN YEAR (2020) NO BUILD AM AND PM PEAK HOUR INTERSECTION LOS ANALYSIS SUMMARY.....7

TABLE 5: DESIGN YEAR (2020) NO BUILD AM AND PM PEAK HOUR ROADWAY SEGMENT LOS ANALYSIS SUMMARY.....7

TABLE 6: EXISTING SPEED LIMITS, POSTED SPEED LIMITS, AND RECOMMENDED DESIGN SPEEDS.....10

TABLE 7: ROADWAY DESIGN CRITERIA.....12

TABLE 8: SFWMD DRAINAGE DESIGN CRITERIA.....13/14

TABLE 9: OSCEOLA COUNTY DRAINAGE DESIGN CRITERIA.....14/15

TABLE 10: ACCESS MANAGEMENT PLAN / INTERSECTION TREATMENTS.....25

TABLE 11: WETLAND AND SURFACE WATER IMPACTS AND MITIGATION COSTS.....29

TABLE 12: BASIN 3A POND SITES SUMMARY.....34

TABLE 13: BASIN 3B POND SITES SUMMARY.....36

TABLE 14: BASIN 5 POND SITES SUMMARY.....38

TABLE 15: BASIN 8A POND SITES SUMMARY.....41

TABLE 16: BASIN 8B POND SITES SUMMARY.....42

TABLE 17: ESTIMATED CONSTRUCTION COSTS SUMMARY.....44

TABLE 18: ESTIMATED RIGHT-OF-WAY COSTS SUMMARY.....44





TABLE 19: ESTIMATED ALTERNATIVES COST SUMMARY.....45
TABLE 20: PARTIN SETTLEMENT ROAD ALTERNATIVES EVALUATION MATRIX.....46

TABLE OF APPENDICES

- APPENDIX A: SHPO REVIEW OF CULTURAL RESOURCE ASSESSMENT SURVEY**
- APPENDIX B: EXISTING RIGHT-OF-WAY AND UTILITY MAPS**
- APPENDIX C: SUMMARY OF RESPONSES TO COMMENTS RECEIVED FROM THE PUBLIC**
- APPENDIX D: PREFERRED ALTERNATIVE PARTIN SETTLEMENT ROAD WIDENING PLANS**



I. INTRODUCTION

The purpose of this report is to summarize the findings, considerations, and recommendations from the planning and conceptual design phase of the Partin Settlement Road Widening project (the study phase has been referred to as “Part A”). After Osceola County’s approval of the typical section and preferred alignment, the final design phase of the project will be initiated (referred to as “Part B”).

During the course of this project, the following reports and surveys were prepared during the Part A process and are referenced as applicable for this report:

- Public Involvement Plan
- Contamination Screening Evaluation Report
- Natural Resources Report
- Geotechnical Report
- Pond Siting Report
- Lighting Justification Report
- Cultural Resource Assessment Survey
- Typical Sections Alternatives Memorandum
- Bridge Hydraulic Study
- Existing Conditions Report
- Existing Right-of-Way and Utility Maps
- Traffic Report
- Public Involvement Summary
- Alternatives Technical Report

Project Background

Partin Settlement Road is located east of the City of Kissimmee in Osceola County and is generally a rural, two-lane roadway with brief sections of a three-lane undivided roadway in several areas.

The project limits extend 2.7 miles from Neptune Road to Lakeshore Boulevard (see Figure 1).



Figure 1: Location Map



The existing posted speed limit varies from 35-45 mph throughout the corridor as indicated in Table 1.

Section	Existing Posted Speed (mph)
Neptune Road to Aeronautical Drive	35
Aeronautical Drive to US 192	45
US 192 to Florida's Turnpike	40
Florida's Turnpike to West of Star Magnolia Drive	40
Star Magnolia Drive to Lakeshore Blvd	35

Corridor Characteristics

There are five existing signalized intersections throughout the project limits. There is also a school zone which serves Partin Settlement Elementary School in the eastern portion of the corridor in front of Northeast Christian Church where the posted speed limit is 40 mph, but changes to 20 mph when flashing.

There are existing sidewalks along the north side of the roadway between Neptune Road and Red Jasper Drive, and between Remington Boulevard and Star Magnolia Drive. However, there is a general lack of adequate pedestrian and bicyclist connectivity throughout the corridor. Several areas are also experiencing heavy traffic congestion (see Section II, Traffic).

Other Related Studies and Projects

Several transportation improvement projects are planned near the study area which are being undertaken by various state and local agencies. These projects were taken into consideration during the development of alternatives for this report. The team identified nearby projects that are seeking or have already received either statewide (Florida DOT STIP) or local (Orlando Urban Area Transportation Improvement Project) allocated funding.

Florida DOT STIP

The 2019 Florida DOT STIP identified one nearby project to receive state funding which will provide \$2 Million for landscaping work on E Bronson Hwy (SR 192) from Denn John Lane to Partin Settlement Road for 2020.

Orlando Urban Area Transportation Improvement Project

The 2019 *Table 1: Existing Speed Limits*

- Florida's Turnpike (FTE) – Adding two variable Toll Express Lanes in each direction from US 192 to Osceola Parkway. This project will include the demolition and reconstruction of the existing Partin Settlement Road Overpass of FTE. The proposed Partin Settlement Road Widening Project will need to continue to coordinate with FTE to ensure that the project will tie-in with the proposed improvements. This project is funded through 2022/2023 for approximately \$279 Million.



- Neptune Road – Widen to four lanes from Partin Settlement Road to US 192. This project is on the Prioritized Project List but is still seeking approximately \$59 Million in funding.
- Neptune Road Phase II – Widen to four lanes from Partin Settlement Road to Neptune Middle School, Eastern Driveway. This project is funded through 2022/2023 for approximately \$45 Million.

Alternatives Development and Methodology

This study has identified various issues in the Partin Settlement Road corridor including traffic growth, congestion, safety, and multi-modal deficiencies, and developed several options to address these deficiencies while improving travel, mobility, and access along the corridor.

As part of the planning and alternatives process, existing conditions have been researched for the project area including roadway context, typical section elements, roadway geometrics, land use, funding, and traffic conditions. Based on this information, potential multi-modal improvements are being considered for the Partin Settlement Road corridor including the use of bicycle lanes and sidewalks.

Based on the findings from Project Traffic Analysis Report (PTAR), additional alternatives have also been developed to meet future traffic demands, reduce congestion, improve safety, and provide connectivity for pedestrians and bicyclists. An evaluation of these alternatives are discussed further in this report.

II. TRAFFIC

A Project Traffic Analysis Report (PTAR) was completed to identify future traffic demands and provide improvement alternatives to address these demands. Based on the forecasted traffic demands, additional lanes are needed which will require widening Partin Settlement Road from a two-lane, undivided roadway to a four-lane section along with improvements to intersections along the corridor (“Build” Alternative) from Neptune Road/CR 525 to Lakeshore Boulevard. A “No-Build” Alternative where no improvements would be undertaken was also evaluated as a part of the study.

Existing Operational Results

Based on the existing intersection analysis, all the study intersections currently operate at an acceptable level of service or better during both AM and PM peak periods, except for the intersection of Partin Settlement Road and US 192. The 2020 existing year intersection analysis shows that the intersection of Partin Settlement Road and US 192 currently operates at Level of Service E during both AM and PM peak periods.

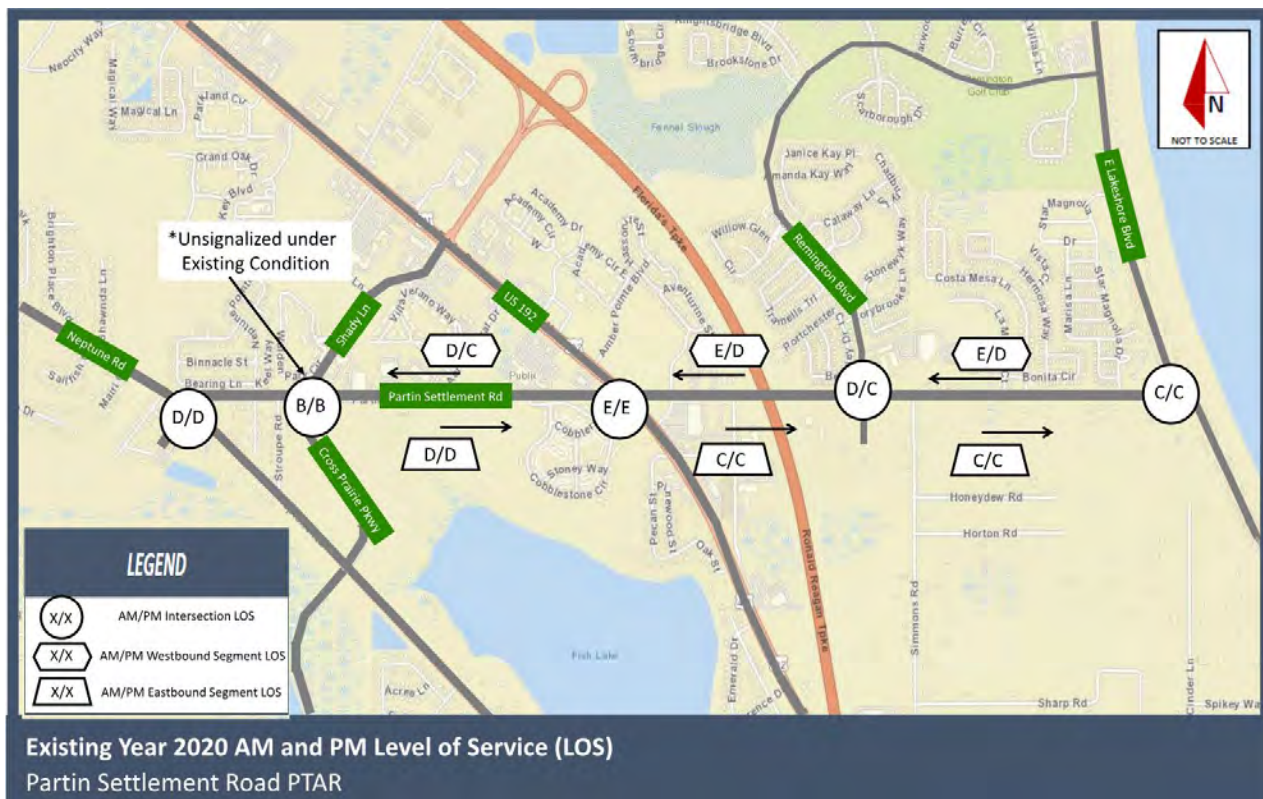


Figure 2: Existing Year 2020 AM and PM Level of Service

Table 2: Existing Year (2020) AM and PM Peak Hour Intersection LOS Analysis Summary

Intersection Along Partin Settlement Road	AM Peak		PM Peak	
	Delay (seconds/vehicle)	LOS	Delay (seconds/vehicle)	LOS
Neptune Road to CR 525 (signalized)	40.9	D	52.9	D
Shady Lane/Cross Prairie Parkway (unsignalized)	12.8	B	12.1	B
US 192 (signalized)	58.3	E	60.5	E
Remington Boulevard (signalized)	37.5	D	28.9	C
Lakeshore Boulevard (signalized)	31.9	C	26.5	C

The 2020 existing year arterial analysis was conducted and the AM and PM peak level of services are provided in Table 3 below. Osceola County currently classifies Partin Settlement Road as a Urban Major Collector.

Based on the existing roadway segment analysis, all of the arterial segments operate at an acceptable level of service during the AM peak period, except for the westbound segments of Partin Settlement Road from US 192 to Lakeshore Boulevard. During the PM peak hour period, all the study segments along Partin Settlement Road operate at an acceptable level of service

Table 3: Existing Year (2020) AM and PM Peak Hour Roadway Segment LOS Analysis Summary

Roadway	Segment	Distance (mi)	AM Peak		PM Peak	
			Arterial Speed (mph)	Road Segment LOS	Arterial Speed (mph)	Road Segment LOS
Partin Settlement Road EB	Neptune Road/CR 525 to US 192	1.20	21.0	D	21.4	D
	US 192 to Remington Boulevard	0.65	27.9	C	26.5	C
	Remington Boulevard to Lakeshore Boulevard	0.86	27.5	C	27.1	C
Partin Settlement Road WB	Lakeshore Boulevard to Remington Boulevard	0.86	17.0	E	20.8	D
	Remington Boulevard to US 192	0.65	13.8	E	18.2	D
	US 192 to Neptune Road/CR 525	1.20	21.8	D	23.2	C

Future Operational Results Under No Build Conditions

The No Build scenario assumes no changes to the existing two-lane roadway or other operational improvements in the future. As shown in Figure 3, Levels of Service for the corridor will degrade substantially with all intersections operating at capacity or failing with high delays. Tables 2 and 3 on the next page provide additional details regarding the No Build scenario.

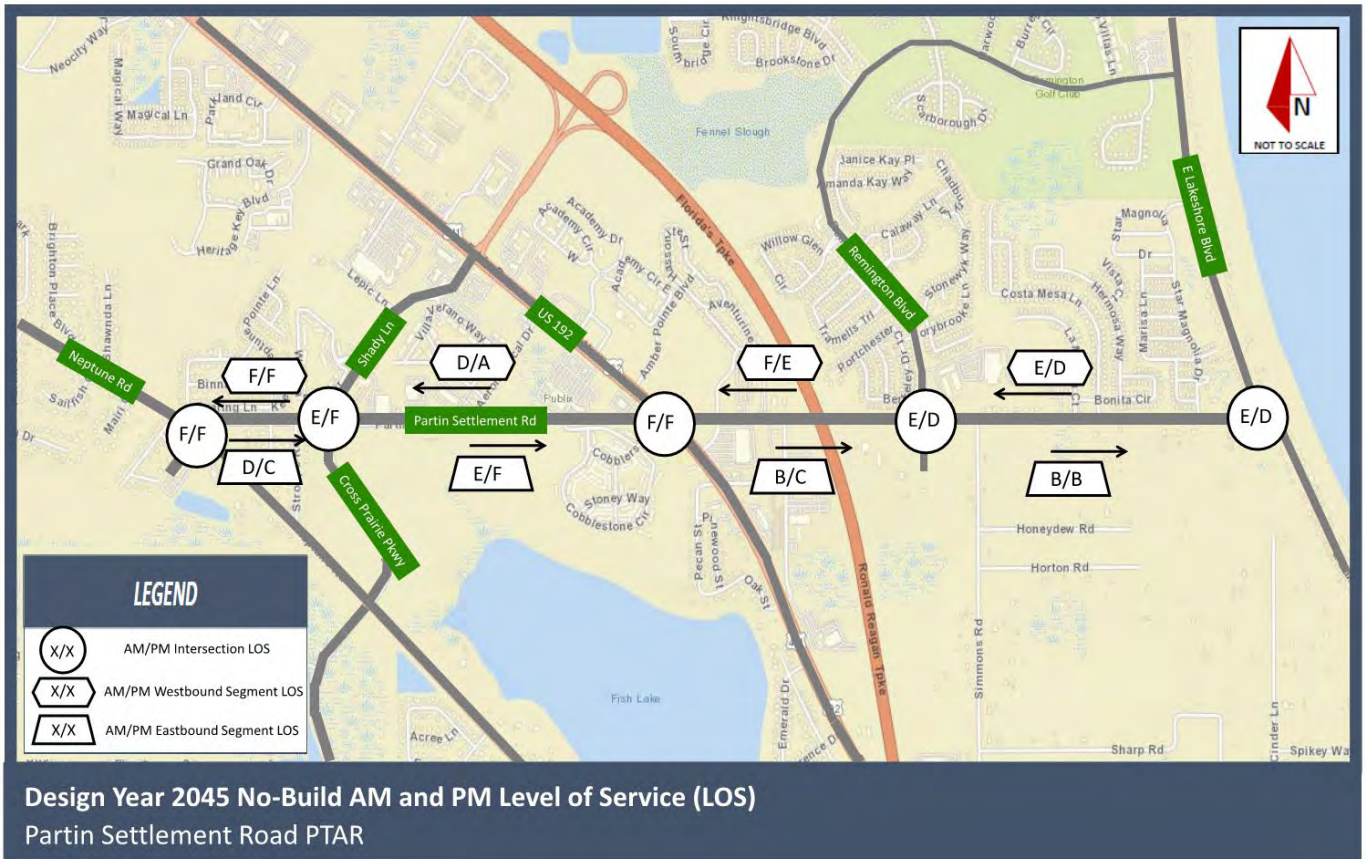


Figure 3: Design Year 2045 No Build AM and PM Level of Service

Table 4: Design Year (2045) No Build AM and PM Peak Hour Intersection LOS Analysis

Intersection Along Partin Settlement Road	AM Peak		PM Peak	
	Delay (seconds/vehicle)	LOS	Delay (seconds/vehicle)	LOS
Neptune Road to CR 525 (signalized)	214.3	F	215.6	F
Shady Lane/Cross Prairie Parkway (unsignalized)	74.2	E	86.5	F
US 192 (signalized)	218.1	F	209.0	F
Remington Boulevard (signalized)	70.9	E	37.7	D
Lakeshore Boulevard (signalized)	55.3	E	35.5	D

Table 5: Design Year (2045) No Build AM and PM Peak Hour Roadway Segment LOS Analysis

Roadway	Segment	Distance (mi)	AM Peak		PM Peak	
			Arterial Speed (mph)	Road Segment LOS	Arterial Speed (mph)	Road Segment LOS
Partin Settlement Road EB	Neptune Road/CR 525 to Shady Lane/Cross Prairie Parkway	0.33	20.8	D	23.5	C
	Shady Lane/Cross Prairie Parkway to US 192	0.87	14.3	E	10.2	F
	US 192 to Remington Boulevard	0.65	32.9	B	27.4	C
	Remington Boulevard to Lakeshore Boulevard	0.86	29.6	B	28.1	B
Partin Settlement Road WB	Lakeshore Boulevard to Remington Boulevard	0.86	16.3	E	20.6	D
	Remington Boulevard to US 192	0.65	9.4	F	16.5	E
	Shady Lane/ Cross Prairie Parkway to US 192	0.87	17.2	D	36.5	A
	Shady Lane/Cross Prairie Parkway to Neptune Road/CR 525	0.33	9.7	F	11.2	F

Safety

The crash data for Partin Settlement Road between CR 525 and Lakeshore Boulevard from 2015 to 2020 was used to analyze the prevalence of crash events in the project area. Over this span, 185 crashes occurred within the study area. Of those 185 crashes, there were 146 crashes (79%) that occurred during the daytime and 39 crashes (21%) outside of daylight conditions (dusk, dawn, and night) as depicted in Figure 4.

Figure 5 shows the locations of the crashes along the Partin Settlement Road corridor. The crash rate for this roadway is 4.940 per million entering vehicles (MEV) through the 2.7 mile segment, which is significantly higher than Florida’s average statewide crash rate of 0.736 MEV for similar undivided roadways from 2012 to 2016.

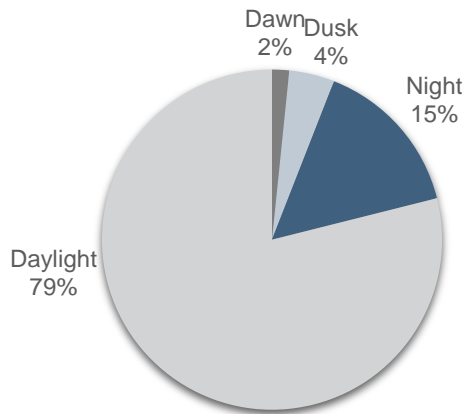


Figure 4: Crash Summary and Associated Lighting Conditions

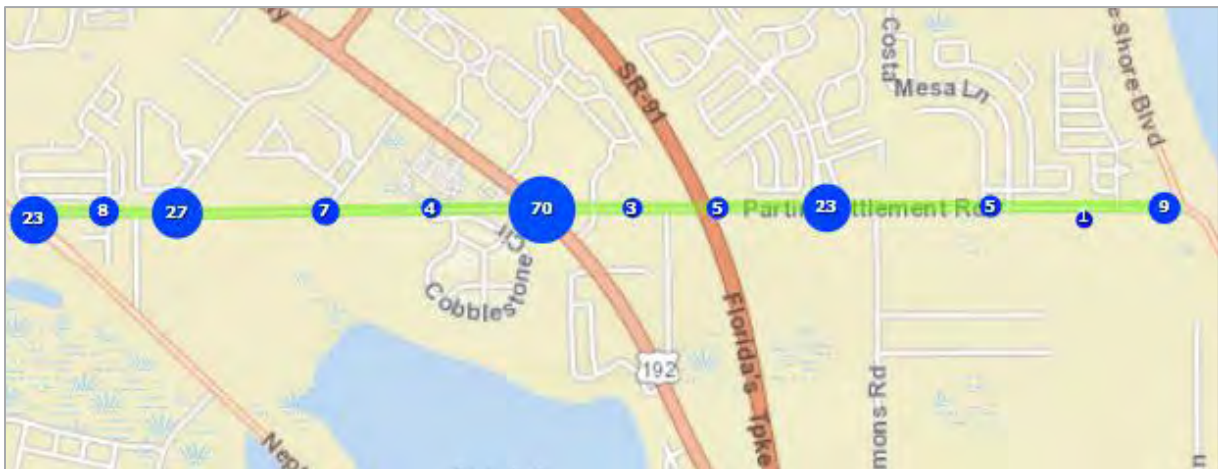


Figure 5: Crash Locations

III. PURPOSE AND NEED

Project Purpose

The purpose of the Partin Settlement Roadway widening improvements is to address various deficiencies in the present corridor including high traffic demands, congestion, lack of multi-modal provisions, safety, drainage, and traffic operations. The existing two-lane section or No Build alternative is not expected to meet future traffic demands nor does it provide for continuous multi-modal accommodations throughout the corridor.

Project Need

Partin Settlement Road between Neptune Road and East Lakeshore Boulevard functions as an important east-west connector. The proposed improvements will address the above deficiencies and provide the following benefits:

- Increase capacity to address traffic growth in the corridor. Traffic volumes are expected to rise substantially over the next 24 years resulting in more delays and lower Levels of Service unless more capacity is provided. The proposed alternatives are expected to improve traffic operations with better Levels of Service as shown on the figure to the right.

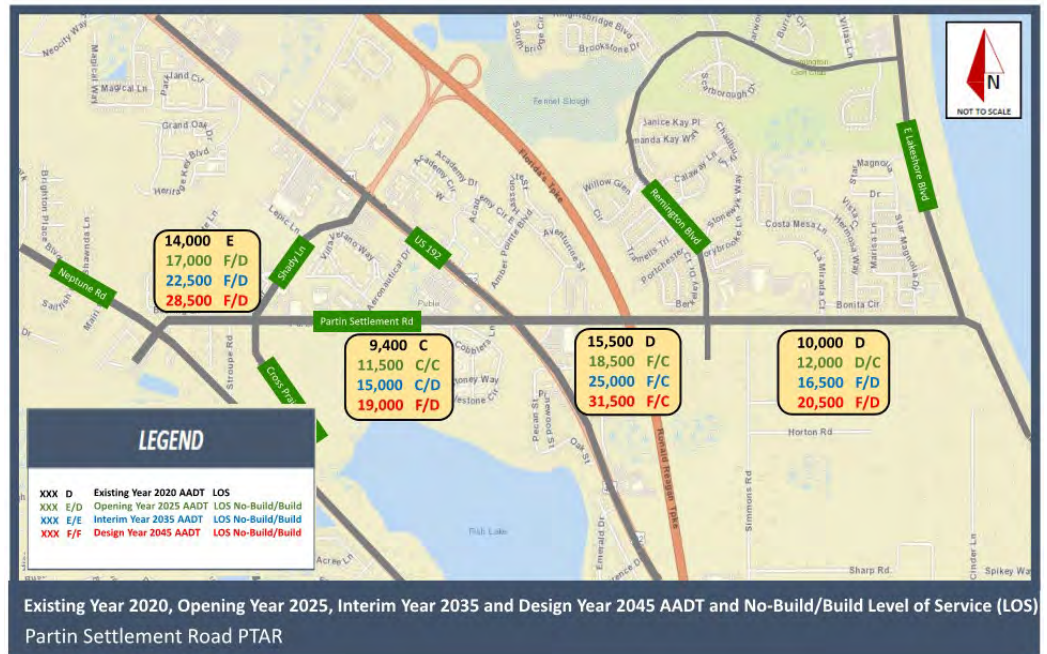


Figure 6: AADT and No-Build/Build Level of Service

- Multi-modal improvements to enable pedestrian and bicycle connectivity through the use of continuous sidewalks.
- Safety features such as a median which is expected to reduce crashes, improve operations, and provide a refuge for pedestrians.
- Enhanced ITS and traffic signals to improve vehicular operations.
- A new, enclosed storm sewer system with treatment ponds to replace the open ditch drainage system.

In summary, the project improvements will align with the goals of Osceola County and address the needs as identified in the traffic and safety analyses.



IV. INITIAL ALTERNATIVES

Design Controls

Various design criteria for the proposed roadway and drainage alternatives are discussed in this section.

Existing Speed Limits and Design Speeds

Existing and proposed design speeds are shown in Table 6. The design speeds indicated below are recommended to provide additional sight distance and safety measures for motorists.

Table 6: Existing Speed Limits, Posted Speed Limits, and Recommended Design Speed Limits

Section	Existing Posted Speed (mph)	Recommended Posted Speed (mph)	Proposed Design Speed (mph)
Neptune Road to Aeronautical Drive	35	35	40
Aeronautical Drive to US 192	45	35	40
US 192 to Florida's Turnpike	40	40	45
Florida's Turnpike to West of Star Magnolia Drive	40	40	45
Star Magnolia Drive to Lakeshore Drive	35	35	40

Typical sections elements from two recently approved projects in Osceola County (Neptune Road and Simpson Road) were considered during the development of the Partin Settlement Road improvements to maintain consistency with other similar County roadways. These elements are described in further detail below.

Lane Widths

Approved typical sections for Neptune Road and Simpson Road indicate the use of 11-foot travel lanes for undivided sections. The Florida Department of Transportation (FDOT) Design Manual (FDM) indicates 10-foot or 11-foot minimum travel lanes for design speeds from 25-35 mph, and 35-45 mph, respectively, for a C3 Suburban context classification. For this project, 11-foot travel lanes are proposed to remain consistent with other County roads in the area and the FDM.

Median Widths

The typical sections for Neptune Road and Simpson Road indicate 22-foot and 26-foot median widths, respectively. The FDM indicates 22-foot minimum median widths for design speeds from 25-45 mph for a C3 Suburban context classification, though FDOT allows median widths in more urbanized applications (Context Classifications C4, C5, and C6) to be reduced to 15.5 feet wide. For this project, the median width may vary depending on the existing right-of-way, and thus may range from a 16-foot median width where the right-of-way is relatively narrow up to a maximum 22-foot median.



Bicycle Facilities

The 2018 Trail Prioritization and Feasibility Report includes provisions for a proposed bicycle facility running parallel to Partin Settlement Road between Neptune Road and Lakeshore Boulevard.

The project team developed two options with on-road bike lanes in each direction (see page 16) and another two options without on-road bike lanes, though 10-foot sidewalks were provided with the latter options to accommodate both bicyclists and pedestrians. In accordance with the 2012 AASHTO Guide for the Development of Bicycle Facilities, at least one of the sidewalks should have a preferred minimum width of 10 feet to accommodate both pedestrians and bicyclists with an absolute minimum width of eight feet. These widths are also consistent with the FDM which allows such shared facilities to substitute for bike lanes on roads with a design speed of 35 mph or greater.

The FDM indicates bicycle lanes can be used on curbed roadways with a design speed ≤ 45 mph, although it is best practice to consider other types of facilities for design speeds greater than 30 mph. Since design speeds for the proposed improvements are in the range of 40-45 mph, the two options without the on-road bike lanes would be in general agreement with the FDM. If on-road bike lanes are utilized, the FDM notes that seven-foot wide buffered bicycle lanes are standard on new construction projects, although four-foot-wide lanes are also allowed in areas where it is not practical to move the curb. In areas with constrained right-of-way, wider sidewalks can provide a higher degree of multi-modal accommodations than on-road bicycle lanes.

Sidewalks

The typical sections for Neptune Road and Simpson Road do not indicate sidewalks on their facilities, though multi-use/shared use paths have been provided instead. The FDM indicates minimum six-foot sidewalks for C3 context classifications.

For this project, a variable sidewalk width is recommended that would allow a six-foot width where the existing right-of-way is relatively narrow up to a maximum 10-foot sidewalk. In addition, the County may also consider placing the sidewalk at the back of curb in areas of constricted right-of-way.

Utility Strip

The typical sections for Neptune Road and Simpson Road indicate utility strips of 10 and 8.5 feet, respectively. The County's code has recently changed which would permit the utility strip to be narrowed further, though still allow for plantings, light fixtures, and signage. Accordingly, a four-foot wide utility strip is recommended for this project, although a two-foot wide utility strip may be provided in constrained areas.

Roadway Lighting

A *Lighting Justification Report* was completed along Partin Settlement Road from CR 525 (Neptune Road) to Lakeshore Boulevard. As part of this report, an evaluation was completed to determine the need for continuous lighting throughout the corridor. The lighting criteria are based on the Florida Department of Transportation's (FDOT) Manual of Uniform Traffic Studies (MUTS), Chapter 14, Roadway Lighting

Justification Procedure. The report indicated the benefit-cost effect of lighting the roadway did not produce the anticipated reduction in crashes during low light conditions, and thus, would not warrant corridor lighting. However, the County's preference is to provide corridor lighting on new facilities with sidewalks and shared-use paths for security and safety reasons.

Roadway Design Criteria

Roadway design criteria for this project are summarized on Table 7 which reflects a combination of County and FDOT standards and criteria.

Table 7: Roadway Design Criteria

Roadway Design Criteria		
Design Element	Design Standard	Design Source
Context Classification	Neptune Road to Aeronautical Drive (C3R-Suburban Residential)	FDM, Part 2, Table 200.4.1
	Aeronautical Drive to Florida's Turnpike (C3C-Suburban Commercial)	FDM, Part 2, Table 200.4.1
	Neptune Road to Aeronautical Drive (C3R-Suburban Residential)	FDM, Part 2, Table 200.4.1
County Classification	Urban Major Collector	Osceola County
Posted Speed	35-45 mph	Osceola County
Proposed Design Speed	40-45 mph	FDM, Part 2, Table 201.5.1
Number of Travel Lanes	4	Per Scope
Minimum Travel Lane Width	11'	FDM, Part 2, Table 210.2.1
Minimum Grade	0.30%	FDM, Part 2, Sect 210.10.1.1
Travel Lane Cross Slope	2%	FDM, Part 2, Figure 210.2.1
Median Width	15.5' – 40 mph, 22' – 45 mph	FDM, Part 2, Table 210.3.1
Bike Lane Width	4' Min, 7' Max	FDM, Part 2, Sect. 223.2.1.1
Min. Sidewalk Width	6'	FDM, Part 2, Table 222.1.1
Sidewalk Cross Slope	2%	FDM, Part 2, Sect 222.2.1.3
Outside Curb and Gutter	Type F	FDM, Part 2, Sect 210.5
Median Curb and Gutter	Type E	FDM, Part 2, Sect 210.5
Roadside Frontslope	1:2 Max, 1:6 Min	FDM, Part 2, Table 215.2.3
Roadside Backslope	1:2 Max, 1:6 Min	FDM, Part 2, Table 215.2.3
Border Width	8' Min	FDM, Part 2, Sect 210.7
Min Lateral Offset	4' from Face of Curb	FDM, Part 2, Table 215.2.2



Drainage Design Criteria

The project is located within the jurisdiction of the South Florida Water Management District (SFWMD) and Osceola County. A permit from the SFWMD will be needed to ensure the design criteria meets the criteria set forth by the SFWMD and Osceola County. A pre-application meeting was held with SFWMD on November 19, 2020 to discuss what SFWMD criteria would be relevant to the project. The following bullets summarize the criteria and pertinent information discussed.

- The project will be designed to provide 2.5 inches of treatment over the net new impervious area.
- Conformance to SFWMD dimensional criteria is not mandatory for linear transportation projects due to the constraints that available right-of-way may impose.
- Phosphorous calculations may be required for only the existing systems being modified that had previously performed those calculations.
- Phosphorous calculations may not be necessary for a linear transportation project unless there is a direct discharge to impaired water.
- The project will be designed to not increase the peak discharge for the 10-year / 72-hour design storm.
- Floodplain impacts will be compensated for with a cup-for-cup analysis.
- Mitigation for wetland impacts will be provided by the purchase of mitigation bank credits.

Additional criteria that is pertinent to the project for SFWMD and Osceola County was found in the SFWMD Environmental Resource Permit Applicant's Handbook Volume II (Effective May 22, 2016) and the Osceola County Land Development Code (Updated September 8, 2020) is tabulated in Table 8 and Table 9, respectively.

Table 8: SFWMD Drainage Design Criteria

SFWMD Drainage Design Criteria	
Category	Description
<u>Treatment Volume</u>	
Treatment Volume	Wet detention volume shall be provided for 2.5 inches over the new impervious area plus any existing treatment volume
Lake Okeechobee BMAP	An additional 50% treatment volume shall be provided for because the project is located within the Lake Okeechobee BMAP.
<u>Bleed Down Device</u>	
Bleed Down Criteria	Devices shall be sized based upon a maximum design discharge of ½ inch of the detention volume in 24 hours.



Bleed Down Device Size	Devices shall incorporate dimensions no smaller than 6 square inches of cross-sectional area, two inches minimum dimension, and 20 degrees for V notches.
<u>Pond Design</u>	
Pond Dimension Criteria	Minimum pond area is 0.5 acres.
	Minimum perimeter maintenance and operation easements of 20 feet width at slopes no steeper than 1:4 shall be provided beyond the control elevation water line.

Table 9: Osceola County Drainage Design Criteria

Osceola County Drainage Design Criteria	
Category	Description
<u>Pond Design</u>	
Maintenance Berm	A 10 ft wide berm with a slope no steeper than 1:10.
Wet Pond Side Slopes	No steeper than 1:4 to 3 ft below control elevation, then no steeper than 1:2 to pond bottom.
Wet Pond Erosion Protection	Side slopes and berms to be sodded from 3 ft below the control elevation up to 5 ft outside toe of berm.
Toe of Slope to Property Line	Toe of berm slope must be 5 ft minimum from the property line.
Control Structure Skimmer	Skimmer shall be installed at an elevation 6 inches below the lowest control structure opening and elevated to an elevation 6 inches above the design highwater level.
Design Storm	10-year / 72-hour Storm.
Peak Stage	Minimum 1 ft of freeboard between the design high water level and minimum berm elevation.
Tailwater	Discharge directly to lakes or rivers and streams: Regulated high water level or modelled design storm elevation.
	Discharge directly to regulated canals: Agency regulated control elevation.
	Pond discharging to an Existing Storm Sewer System: Elevation of the HGL of the existing system at the connection for the design storm event.
<u>Storm Sewer</u>	
Design Storm	Designed to 10-year frequency.
HGL	The calculated hydraulic gradient line shall be at or below the design elevation of the gutter.



Inlet Capacity	Curb Inlet Type 1 and 3 peak inflow to inlet is 5 cfs in the 10-yr storm. Curb Inlet Type 2 and 4 peak inflow to inlet is 9 cfs in the 10-yr storm.
Minimum Pipe Size	Minimum storm sewer pipe is 18 inch, minimum side drain pipe is 15 inch, and minimum outfall pipe is 15 inch.
Pipe Grade	Minimum velocity is 2.5 fps when flowing full.
Pipe Discharge Velocity	When outlet velocities for the design of storm discharges exceed six (6) fps, the need for special channel lining or energy dissipation is required.
Maximum Lengths of Pipe	Maximum length of 15 inch pipe is 200 ft.
	Maximum length of 18 inch pipe is 300 ft.
	Maximum length of 24 to 36 inch pipe is 400 ft.
	Maximum length of 42 inch pipe and larger is 500 ft.
Spread	If Design Speed less than or equal to 45 mph, then keep 1/2 of lane clear.
	If Design Speed greater than 45 mph and less than or equal to 55 mph, then keep 8' of lane clear.
	If Design speed greater than 55 mph, then no encroachment into lane.
Tailwater	Storm Sewer System discharging to Pond: Pond water surface elevation at the point of peak inflow during the 10-yr / 24-hr storm event.
	Storm Sewer System discharging to an Existing Storm Sewer System: Elevation of the HGL of the existing system at the connection for the design storm event.
<u>Ditches</u>	
Minimum Slope	Minimum roadside ditch slope shall be 0.1%.
Design Storm	Designed to 10-yr frequency.
Side Drains	Designed to 10-yr frequency.
<u>Cross Drains</u>	
Design Storm	Designed to 50-yr frequency.

Description of Typical Section Options

Using the roadway classifications and the County’s future transportation and land use plans, the project team developed and evaluated various typical as described below. Each typical section under consideration also included lighting, signal, and ITS improvements to enhance safety at signalized intersections along the roadway. As part of the roadway improvements, drainage will be accommodated by an enclosed storm sewer system to replace the existing open ditched currently found along the corridor

Option 1A: Four-lane Divided Urban Roadway with Median

This option consists of a four-lane divided urban roadway with 11-foot travel lanes, a 16-foot median, and 10-foot sidewalks on both sides of Partin Settlement Road. The proposed utility strip is two feet wide.

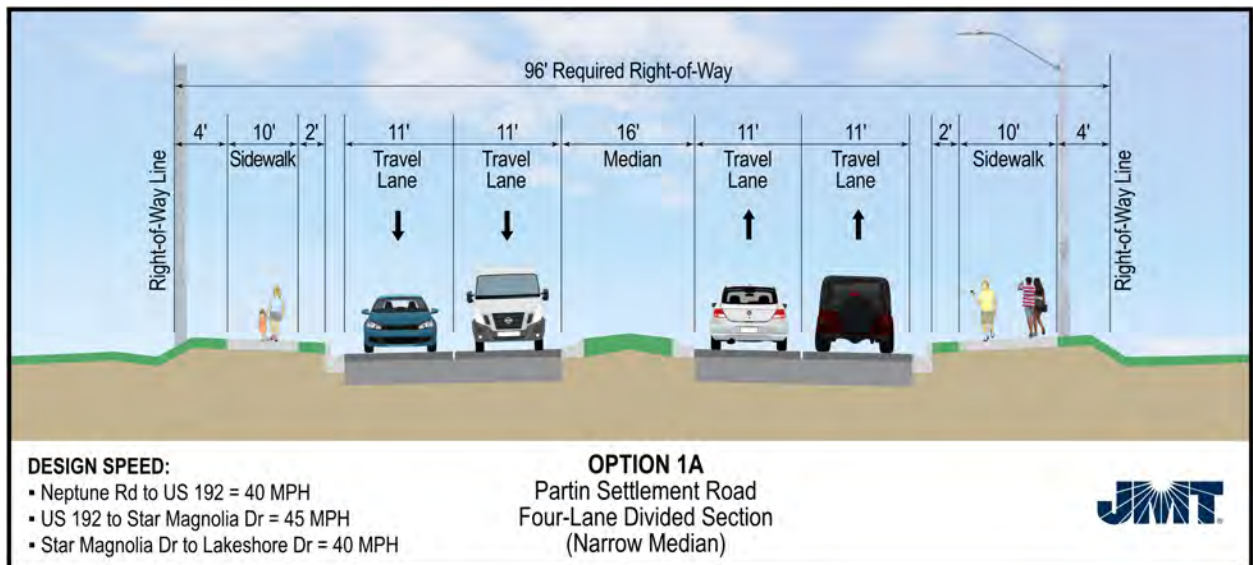


Figure 7: Typical Section Option 1A

Option 1B: Four-lane Divided Urban Roadway with Median and On-Road Bicycle Lanes

This option consists of a four-lane divided urban roadway with 11-foot travel lanes, a 16-foot median, and four-foot on-road bicycle lanes. The sidewalks would be placed at the back of curb and consist of a six-foot sidewalk on the north side of Partin Settlement Road, and an eight-foot sidewalk on the south side of this roadway. Both Options 1A and 1B would require a 96-foot right-of-way.

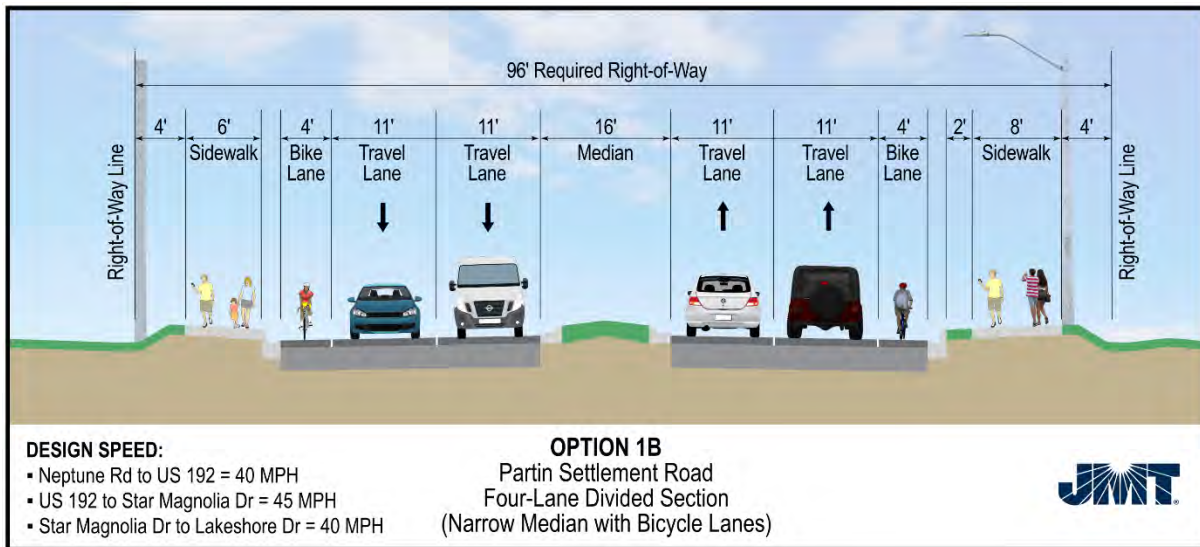


Figure 8: Typical Section Option 1B

Option 2: Four-lane Divided Urban Roadway with Wider Median and On-Road Bicycle Lanes

This option consists of a four-lane divided urban roadway with 11-foot travel lanes, a 22-foot median, and four-foot on-road bicycle lanes. In addition, a six-foot sidewalk would be provided on the north side of Partin Settlement Road while the sidewalk on the south side of the road would be 10-feet wide. The proposed utility strips are two-feet wide.

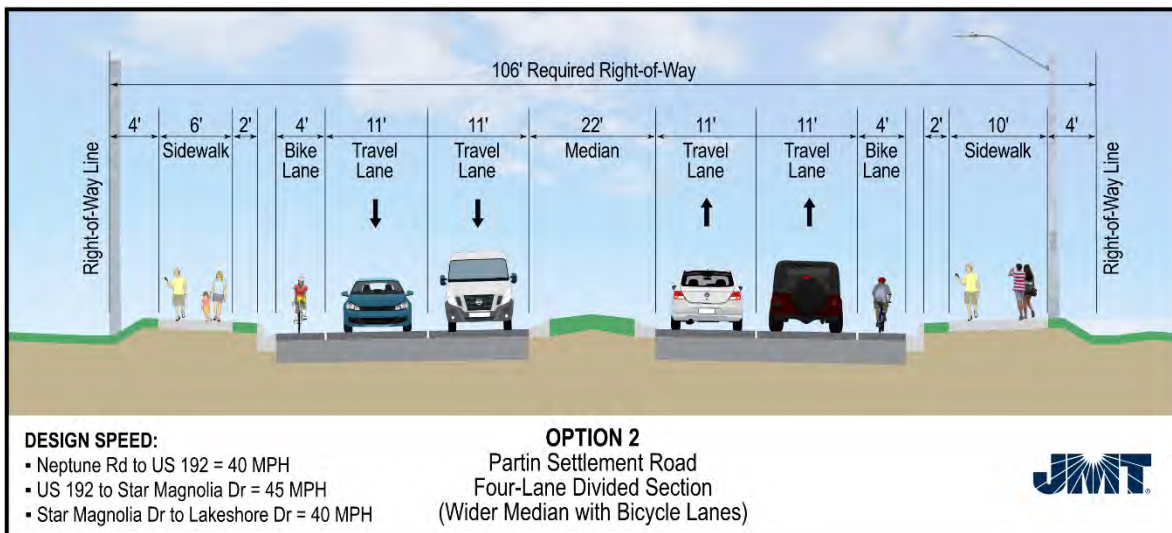


Figure 9: Typical Section Option 2

Option 3: Four-lane Divided Urban Roadway with Wider Median

This option consists of a four-lane divided urban roadway with 11-foot lanes and a 22-foot median. Also, 10-foot sidewalks would be built on both sides of Partin Settlement Road and the proposed utility strips are four-feet wide. Both Options 2 and 3 would require a 106-foot right-of-way.

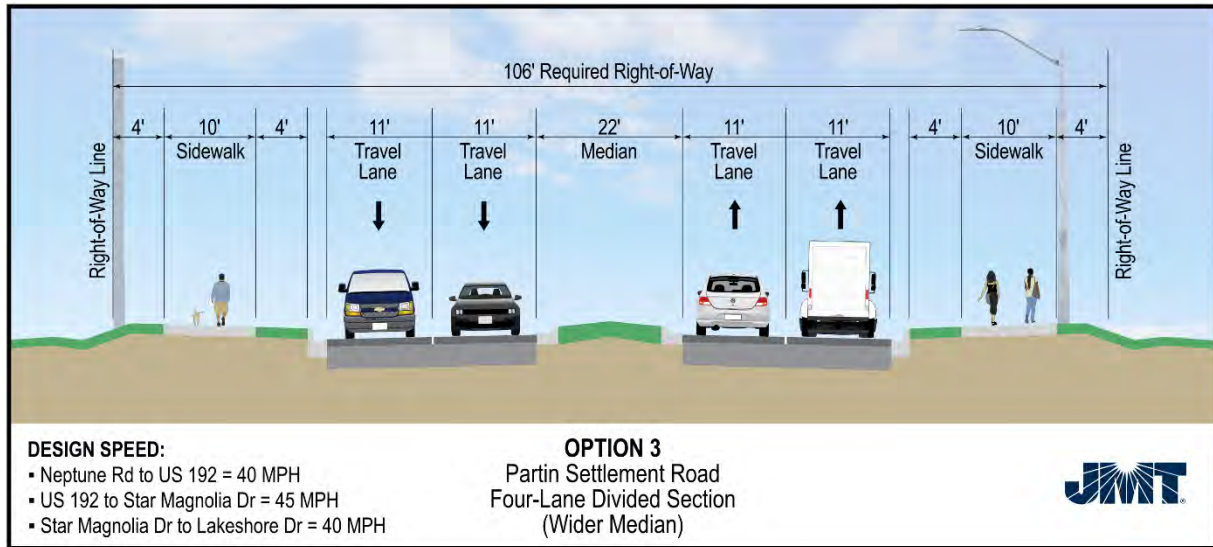


Figure 10: Typical Section Option 3

Option 4: Five-Lane Undivided Urban Section with Two-Way Center Turn Lane (TWCTL)

This option would provide a five-lane undivided urban section with the center lane utilized for left turn movements. This type of section typically has a higher crash rate resulting from motorists traveling in opposite directions and trying to occupy the two-way left turn area at the same time.

Also, TWCTL sections are typically more appropriate for low-speed roadways with many driveways, thus this section would not be applicable to this corridor, due to higher speed travels along Partin Settlement Road. For these reasons, this typical was eliminated from consideration.

Option 5: Urban High-Speed Section

A high-speed section was evaluated, though it was dropped from consideration as it was not consistent with the urbanized character of the Partin Settlement corridor, nor meet the County’s desire to promote multi-modal opportunities for pedestrians and bicyclists.

Higher travel speeds associated with this section would typically discourage all but the most advanced bikers to use on-road bicycle lanes, even with the use of buffered bicycle lanes. Further, high speed urban typical sections are generally more appropriate in areas with high existing posted speed limits and relatively low-density land uses (existing and future) and are not applicable to this corridor.



Option 6: Incorporating Buffered-Bike Lanes with No Shared-Use Path

This option could be applied to the four-lane divided urban section and would provide five-foot bike lanes with a two-foot buffer between the bike lane and the travel lane. This option was considered and dropped based on the desire to have bicycle facilities that better fit a wide range of potential users.

This section would discourage all but the most advanced riders to use the on-road bicycle lanes, even with the buffer between the travel lanes. By providing a separate bicycle trail as well as on-road bike lanes, all bicyclist profiles can be met.

Option 7: Rural Roadway Section

This option would provide a rural typical section consisting of travel lanes, shoulders, and open drainage ditches. Sidewalks can be provided with this section, although they would be placed outside the open ditches for safety reasons.

These types of roadways are not consistent with other recent County roadway improvements and are not conducive to providing multi-modal accommodations for pedestrians and bicyclists in an urban environment. In general, these roadways will likely require more right-of-way along the corridor to address drainage requirements, though the overall right-of-way requirements may be the same since the need for ponds can be reduced or eliminated by using the ditches for treatment. Given these considerations, the rural roadway section is not recommended for this project.

Description of Roadway Alignment Alternatives

In addition to the typical section options, the study also investigated two alignment alternatives consisting of holding the north right-of-way line and widening to the south, and conversely, holding the south right-of-way line and widening to the north. The following text discusses the alignment alternatives in greater detail.

Typical Section Options 1A and Option 1B, (Hold North Right-of-way) (96 Feet of Required Right-of-way)

The design will focus on holding the right-of-way line along the north side of Partin Settlement Road with widening towards the south.

For both Typical Section Options 1A and 1B, 96 feet of right-of-way is required. At designated project intersections, the 16-foot wide median will be designed to provide left turn bays with a traffic separator without increasing the overall median width. At the Shady Lane as well as the Remington Boulevard intersections, the proposed widening will be increased to accommodate dual left turn lanes as well as right turn lanes.

For the Typical Section Options 1A and 1B, a total of 30 properties are anticipated to be impacted by this alternative, with 24 of these impacted properties being located south of the proposed roadway. The impacts



to the six properties on the north side of the roadway are located near the US 192 and Remington Boulevard intersections and are required to accommodate turn lanes at the intersections.

Typical Section Options 2 and Option 3, (Hold North Right-of-way) (106 Feet of Required Right-of-way)

Similar to the above options, this design will also focus on holding the right-of-way line along the north side of Partin Settlement Road with widening towards the south right-of-way line. Typical Section Options 2 and 3 require an additional 10 feet of right-of-way over that needed for Typical Section Options 1A and 1B.

At designated project intersections, the 22-foot wide median will be designed to provide left turn bays with a traffic separator without increasing the overall median width. At the major intersections of Shady Lane and Remington Boulevard, the proposed widening will be increased to accommodate dual left turn lanes as well as right turn lanes.

A total of 36 properties are anticipated to be impacted by Typical Section Options 2 and Option 3 and holding the north right-of-way line. An estimated 29 properties located south of the project are expected to be impacted being. The seven additional properties that will be impacted on the north side of the road are located at either the US 192 or Remington Boulevard intersections.

Figures 11 and Figure 12 have been developed on the next page to indicate the right-of-way impacts for Typical Section Options 1A and 1B (shown in yellow) as well as the needed right-of-way for Typical Section Options 2 and 3 (shown in red shading).

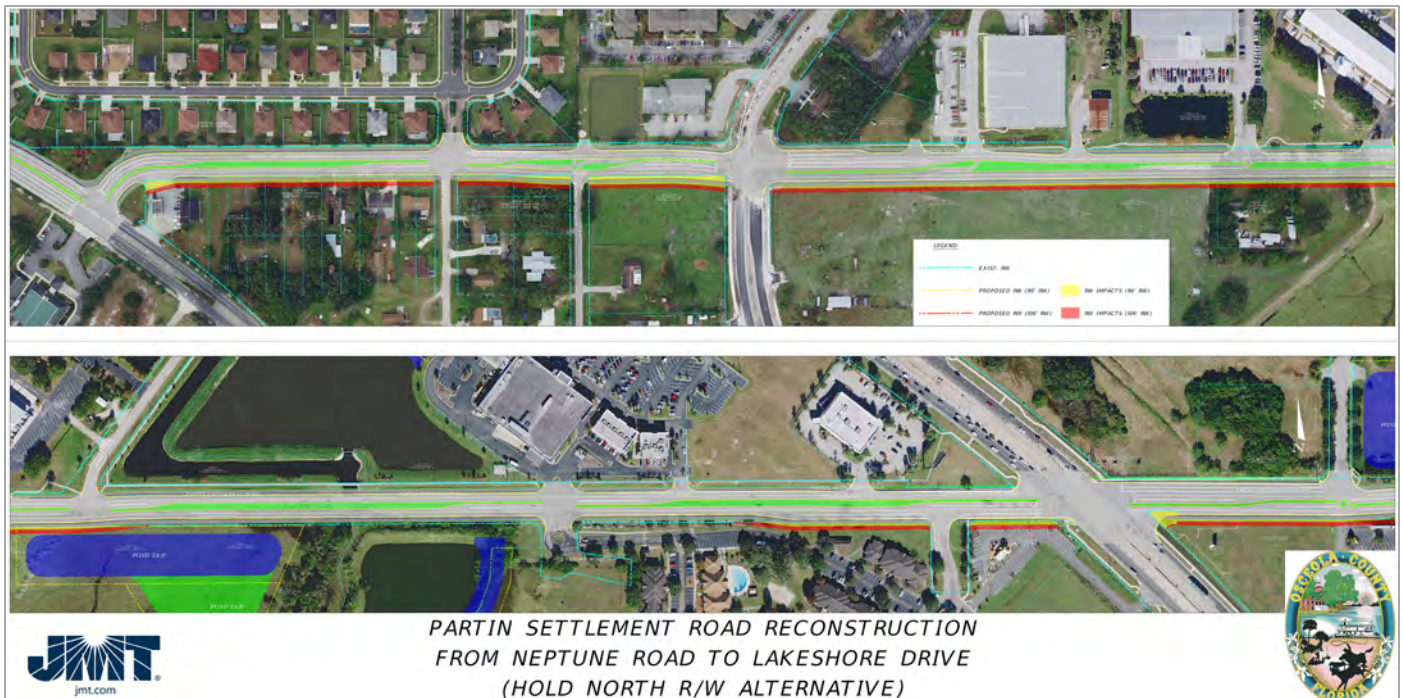


Figure 11: Alignment Alternatives Holding North Right of Way Line – Western Project Limits



Figure 12: Alignment Alternatives Holding North Right of Way Line – Eastern Project Limits



Typical Section Options 1A and 1B (Hold South Right-of-way)(96 Feet of Required Right-of-way)

The design will focus on holding the right-of-way line along the south side of Partin Settlement Road and widening towards the north right-of-way line. Due to development constraints, the section of the project between Neptune Road and Shady Lane will continue to hold the north right-of-way line similar to the earlier discuss for the north Right-of-way Alignment Alternative discussed illustrated on the previous page. In addition, to avoid impacts to the Amber Point development located at the northeast corner of the US 192 / Partin Settlement intersection, the alignment will also hold the north right-of-way line through this development.

For both Options 1A and 1B, 96-foot right-of-way is required. At designated project intersections, the 16-foot wide median will be designed to provide left turn bays with a traffic separator without increasing the overall median width. At the major intersections of Shady Lane and Remington Boulevard, the proposed widening will be increased to accommodate dual left turn lanes as well as right turn lanes.

A total of 39 properties are anticipated to be impacted by this alternative. With 20 of these impacted properties being located north of the proposed roadway. The properties on the south side of the roadway that are impacted are primarily located between Neptune Road and east of the Shady Lane intersection. In addition, three properties near the intersection with US 192 that are located on the south side of the roadway will be impacted. Figures 13 and 14 show the right-of-way impacts for the Typical Section Options 1A and 1B holding the south right-of-way line with the estimated right-of-way impacts shown in yellow shading.

Typical Sections Option 2 and Option 3, (Hold South Right-of-way)

Similar to the above options, this design will also focus on holding the right-of-way line along the south side of Partin Settlement Road with widening towards the north right-of-way line. Typical Section Options 2 and 3 require an additional 10 feet of right-of-way over that needed for Typical Section Options 1A and 1B.

At designated project intersections, the 22-foot wide median will be designed to provide left turn bays with a traffic separator without increasing the overall median width. At the major intersections of Shady Lane and Remington Boulevard, the proposed widening will be increased to accommodate dual left turn lanes as well as right turn lanes.

A total of 39 properties are anticipated to be impacted by this alternative, of which, 20 properties are located north of the proposed roadway. Most of the impacted properties located south of the roadway are in between Neptune Road and east of the Shady Lane intersection. In addition, three properties located on the south side of the roadway at the intersection with US 192 will be impacted. Figures 13 and 14 show the right-of-way impacts in red shading for the Typical Section Options 2 and 3 with the existing south right-of-way line held.

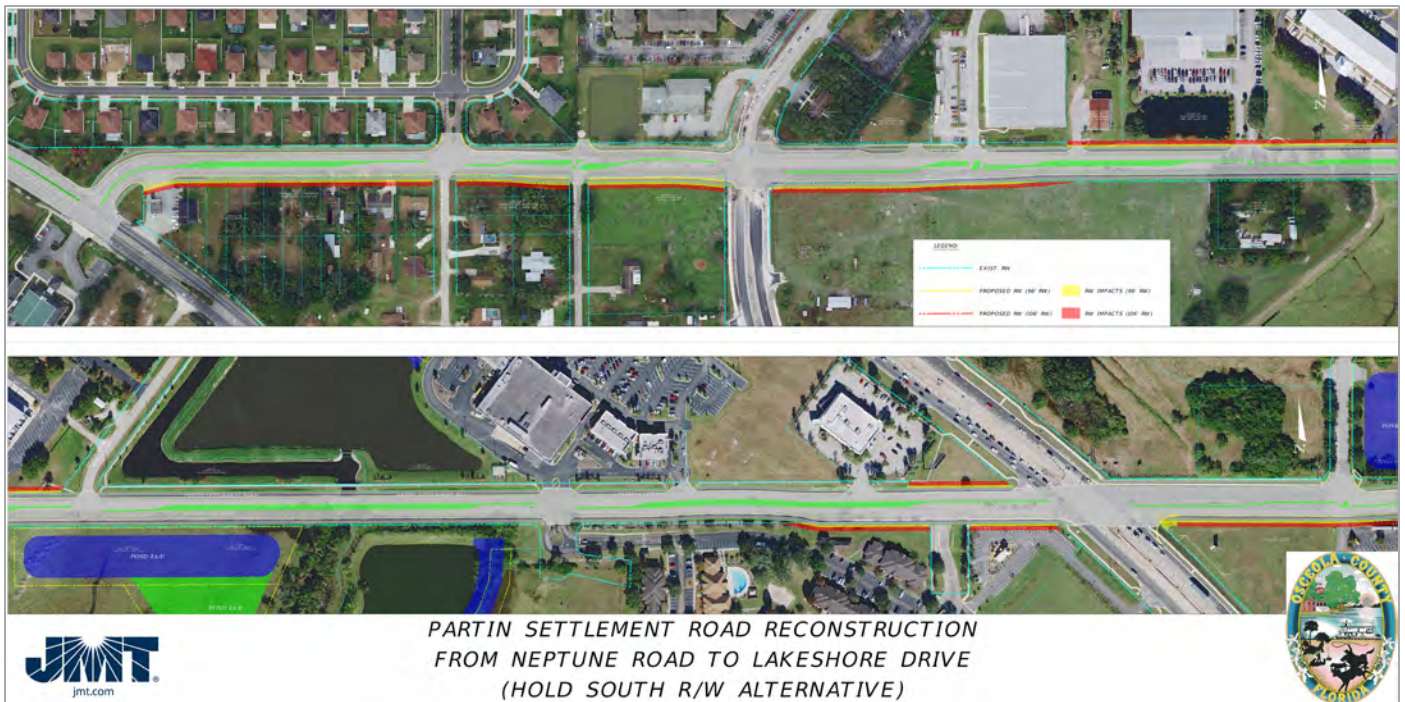


Figure 13: Alignment Alternatives Holding South Right of Way Line – Western Project Limits

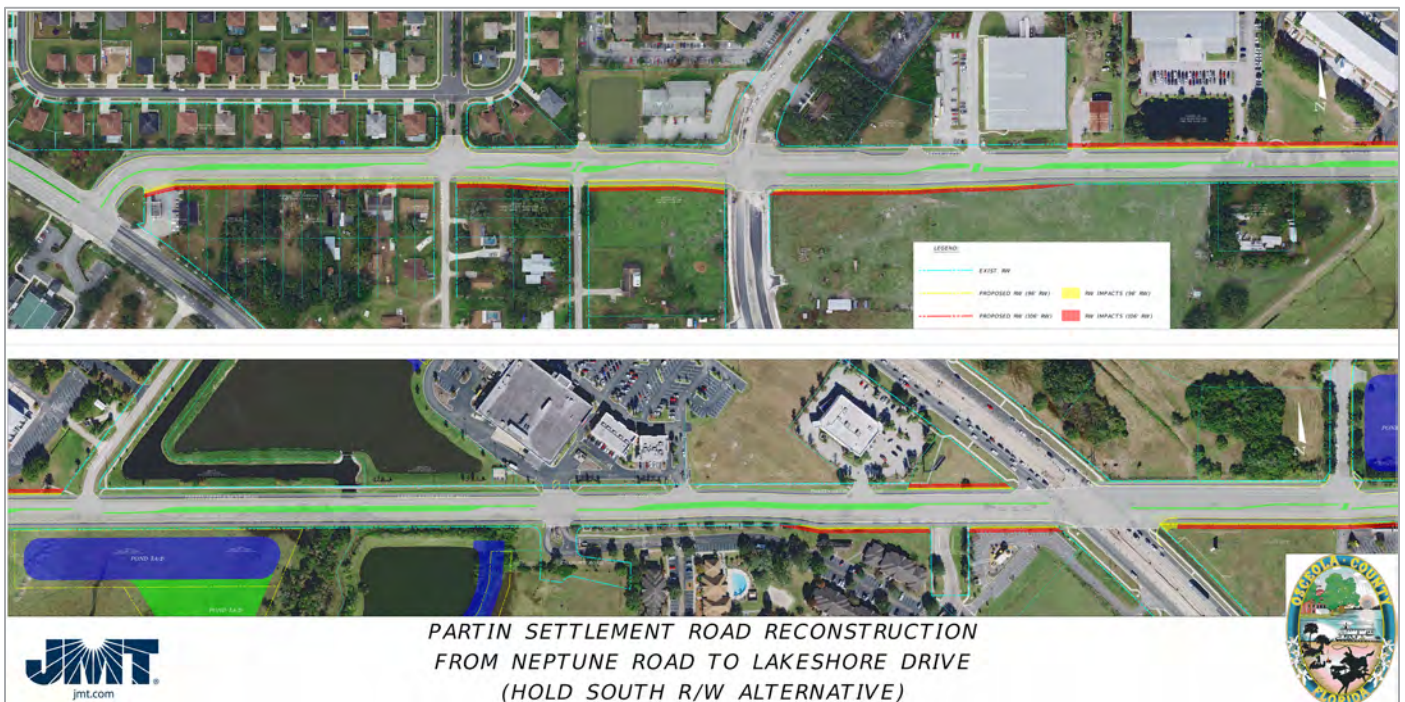


Figure 14: Alignment Alternatives Holding South Right of Way Line – Eastern Project Limits

Access Management

The typical sections for this project incorporate medians as part of the overall proposed improvements. The FDOT Access Management Guidebook (AMG), November 2019, states that medians provide significant safety benefits. These benefits include reduced conflicts, particularly by removing left turning vehicles from the through traffic lanes. Medians also provide a refuge for pedestrians crossing the roadway. The benefits of deploying raised or restrictive medians on multi-lane improvements are so persuasive that FDOT requires median treatments on multi-lane roads with a design speed of 45 mph or greater. These same safety benefits would also apply to roads with design speeds below 45 mph.

With the introduction of medians, openings are needed to provide access to abutting properties, as well as serve cross street traffic. The AMG identifies two types of median opening as well as spacing guidelines between these openings (see Table 3 in the AMG):

- Full median openings - this type of opening allows access for all movements and is typically found at signalized intersections, or at developments which generate relatively high amounts of traffic.
- Directional median openings – these treatments typically apply to low volume applications and provide access from the main road to the side street or business, but do not allow left turns to be made from the side street or business onto the roadway.

The FDOT Access Management Guidebook (AMG) provides spacing guidelines for various types of roadway classes ranging from high-volume urban and rural typical sections to low volume roadways. For Partin Settlement Road, Class 5 is preferred by the County to obtain operational benefits.

For Class 5 designations, the AMG recommends spacing of full intersections at 1,320 feet minimum, while the minimum spacing for directional intersections is 660 feet. For this project, directional treatments are recommended at several T-intersections and the design of these openings would also provide for left turn bays in each direction to allow for U-turns.

An analysis was undertaken for Partin Settlement Road to review access needs in the corridor. Primary cross streets with signalized intersections are considered to warrant a full median opening.. Secondary intersections which provide access to large residential developments and schools were also identified as candidates for full median opening treatments. Another analysis was then undertaken to identify other side streets or access points where directional median openings could be provided to improve access along the corridor while maintaining consistency with the AMG.

The resulting median opening location analysis and median treatment recommendations are shown on Table 10. The analysis found that the current spacing of existing primary and secondary intersections is generally consistent with FDOT's designation of Roadway Class 5 except as noted in Table 10.

Table 10: Access Management Plan / Intersection Treatments

Intersection	Intersection Type	Distance Between Intersections (ft)	Meets FDOT Spacing
Neptune Road (Signalized)	Full		
		1,000	No
Neptune Pointe Lane/Schooner Drive	Full		
Stroupe Road	Right In, Right Out		
		800	No
Shady Lane (Signalized)	Full		
		600	Yes
Walden Park Circle	Right In, Right Out		
		700	Yes
Ice Factory Drive	Right In, Right Out		
		620	Yes
Aeronautical Drive	Full		
		1,240	No
Cobblestone Circle	Full		
		1,380	Yes
US 192 (Signalized)	Full		
		680	Yes
Red Jasper Drive	Directional		
		690	Yes
Gateway Baptist/County Drive	Full		
		2,110	Yes
Remington Boulevard (Signalized)	Full		
		740	Yes
Simmons Road	Directional		
		1,110	Yes
Northeast Christian Church / Partin Settlement Elementary Secondary Access	Directional		
		530	No
Costa Mesa Lane	Full		
		1,320	Yes
Star Magnolia Drive	Directional		
		840	Yes
Lakeshore Drive (Signalized)	Full		

*The FDOT Access Management Guidebook Table 3 – Spacing Guidelines for Roadway Class 5 recommends 1,320 feet of spacing of full intersections, and a spacing for directional intersections of 660 feet.



Structures

Each typical section option proposed for this project is rated roughly equal with each other and can be feasibly designed and constructed. The existing pedestrian bridge (see Figure 15) to the west of US 192 can be accommodated by all of the alignment and typical section alternatives under consideration.

The proposed Partin Settlement Road improvements will also connect with the Florida's Turnpike Enterprise (FTE) planned overpass replacement project that is currently in design.



Figure 15: Pedestrian Bridge

The new Turnpike structures will include two bridges, each with four 11-foot travel lanes, 22-foot median, 5'-4" shoulders on each side of the travel lanes which can also serve as bicycle lanes, and 10-foot wide sidewalks. These bridge improvements are intended to be added to the County's Partin Settlement Road plans as a goes-with project. All of the proposed Partin Settlement Road typical section options are essentially compatible with the proposed FTE improvements with minor differences in sidewalks and provisions for bicyclists that can be addressed with transitions.

In addition, all typical section options will require improvements to the existing Fennel Slough concrete box culvert. The culvert drains Fennel Slough to Fish Lake using dual 5' x 11' (rise x span) concrete box culverts. The existing culvert will need to be extended due to the roadway widening and the installation of a median.

ITS Improvements

ITS improvements consisting of new fiber optic cables, cameras, and other devices will be included as part of the proposed Partin Settlement Road widening. All of the proposed typical sections can accommodate the anticipated ITS elements including the use of Arterial Dynamic Message Signs (ADMS) if desired by the County.

Maintenance of Traffic

All of the proposed typical sections can be built using a similar phasing scheme. The initial phase will include the construction of drainage ponds and two new travel lanes in one direction while traffic is carried on the existing pavement. After this phase is completed, traffic will be shifted to the new travel lanes and the existing pavement will be reconstructed to accommodate the new travel lanes in the opposite direction. Median work will be completed last.

Landscaping Opportunities

Landscaping opportunities can be accommodated in the medians and behind the sidewalks. It is recommended that drought tolerant landscaping be provided without the need for irrigation. For the median landscaping, the size and density of the plants should be carefully selected to avoid potential sight distance issues.



V. ALTERNATIVES EVALUATION

Alternatives Evaluation

This section discusses the evaluation of the typical section and alignment alternatives as presented in Section IV for the proposed Partin Settlement Road improvements. The following text provides a comparison of the four typical section options and summarizes their ability to meet the purpose and need for this project.

Alignment Alternatives, Typical Section Option 1A

This option will fulfill the purpose and need for this project by providing improved pedestrian and bicycle connectivity with 10-foot sidewalks along both sides of the corridor. Traffic congestion along Partin Settlement Road will be reduced by increasing the total lanes by widening the existing two-lane section to a four-lane roadway. The 16-foot median will improve vehicular safety by providing physical separation between opposing travel lanes as well as a refuge for pedestrian crossings.

Additionally, lighting, signal and ITS improvements will be provided to enhance safety and traffic operations at signalized intersections along the roadway. The closed roadway drainage system will include stormwater treatment ponds. This typical along with Option 1B has the least right-of-way impacts.

Alignment Alternatives, Typical Section Option 1B

This option will fulfill the purpose and need for this project by providing improved pedestrian and bicycle connectivity through the use of a six-foot sidewalk on the north side and an eight-foot sidewalk on the south side of the corridor. This option will also provide four-foot on-road bicycle lanes on both sides of the roadway which further achieves the need for bicycle connectivity for the community. Traffic congestion along Partin Settlement Road will be reduced due to widening the existing two-lane section to a four-lane roadway. The 16-foot median will improve vehicular safety by providing physical separation between opposing travel lanes as well as a refuge for pedestrian crossings.

Additionally, lighting, signal and ITS improvements will be provided to enhance safety at signalized intersections along the roadway. The closed roadway drainage system will include stormwater treatment ponds. This typical along with Option 1A has the least right-of-way impacts.

Alignment Alternatives, Typical Section Option 2

This option will fulfill the purpose and need for this project by providing improved pedestrian and bicycle connectivity through the addition of a six-foot sidewalk along the north side of the roadway and a 10-foot-wide sidewalk on the south side of the roadway.

This alternative also provides four-foot on-road bicycle lanes on both sides of the roadway which further achieves the need for bicycle connectivity for the community. Traffic congestion along Partin Settlement Road will be reduced due to widening the existing two-lane section to a four-lane roadway. The 22-foot



median will improve vehicular safety by providing physical separation between opposing travel lanes as well as a refuge for pedestrian crossings.

Additionally, lighting, signal and ITS improvements will be provided to enhance safety at signalized intersections along the roadway. The closed roadway drainage system will include stormwater treatment ponds. This option has the higher right-of-way impacts and costs than Typical Sections 1A and 1B.

Alignment Alternatives, Typical Section Option 3

This option will fulfill the purpose and need for this project by providing improved pedestrian and bicycle connectivity through the addition of 10-foot sidewalks along both sides of the roadway. Traffic congestion along Partin Settlement Road will be reduced due to widening the existing two-lane section to a four-lane roadway. The 22-foot median will improve vehicular safety by providing physical separation between opposing travel lanes as well as a refuge for pedestrian crossings.

Additionally, lighting, signal and ITS improvements will be provided to enhance safety at signalized intersections along the roadway. The closed roadway drainage system will include stormwater treatment ponds. This option has the higher right-of-way impacts and costs than Typical Sections 1A and 1B.

Environmental Evaluation

Natural Resources

Wetland and Surface Water Impacts and Mitigation

A Natural Resources Assessment for Partin Settlement Road Improvements was completed in October 2020 and was documented in the Existing Conditions Report submitted to Osceola County in November 2020. Based on desktop review of available aerial imagery, land use/cover, soil, topographic, and wetland/surface water data, the potential wetlands and surface waters within the project corridor were identified.

In September 2020, a field reconnaissance of the wetland and surface water locations was performed in the preliminary desktop review to verify and generally characterize the habitats within the project corridor. All existing wetlands in the project area that are impacted by the improvements have been previously impacted by other actions such as development activities. Notably, some of the impacts were caused by farming or ranching activities associated with orange groves or cattle pastures. Wetland quality remains moderate, though, as hydric characteristics including soils continue to indicate saturation, and wetland vegetation persists, although much of it comprises invasive and opportunistic species.

Potential impact acreages to wetland and surface waters within the project corridor were estimated based on the alternative road alignment designs, as well as the impacts associated with the proposed pond site alternatives. The estimated impact acreages were used to determine potential mitigation costs that may be associated with the project for each alternative based on each activity proposed.

Potential Uniform Mitigation Assessment Method (UMAM) scores were conservatively estimated and need to be field verified and reviewed by the regulatory agencies during the permitting process. UMAM scores for

the existing “No Build” conditions were given estimated functional quality scores of 8 for wetlands and 6 for surface waters within the project corridor. UMAM scores for the proposed “Build Project” conditions were given estimated functional quality scores of 0 for impacted wetlands within the proposed road right-of-way (ROW) alignments and 3 for impacted wetlands within the proposed pond footprints, since the ponds would still be providing some functional quality. Furthermore, impacted surface waters within the proposed road right-of-way are estimated to receive a score of 4 since most of the existing surface water features within the ROW would be replaced following the realignment of the roadway.

The mitigation costs included in the alternatives report are subject to change based on a formal wetland delineation and UMAM assessment during final design, and the estimated mitigation costs provided here are intended for interpretation of the relative costs among the alternatives. The estimated mitigation cost for each option includes the sums of mitigation required for each alternative and Ponds 3A(1), 3B(2) and 8B(2). Additionally, a total cost of \$5,610 is included as the anticipated permit application fee for an Environmental Resource Permit (ERP) from the South Florida Water Management District (SFWMD). An ERP would be required from the SFWMD for all the proposed alternatives.

Also, due to the influence of, and connectivity of parts of the project to Fish Lake, a federal dredge and fill permit (Clean Water Act Section 404) for impacts to wetlands and surface waters may be required from the Florida Department of Environmental Protection (FDEP) under the new state assumption of the 404 program. Typically, a federal 404 permit would not require an application fee when the United States Army Corps of Engineers (USACE) was the issuing authority. However, since the new state assumption of the 404 program was only recently approved (December 17, 2020), it is not known at this time if and/or how much the permitting fee would cost for federal authorization. Thus, no permitting fee for federal 404 authorization is included in the total estimated costs for the proposed alternatives.

At this time, no species-oriented permitting is anticipated. However, a gopher tortoise survey is planned during final design in accordance with the Florida Fish and Wildlife Commission (FWC) guidelines.

Table 11: Wetland and Surface Water Impacts and Mitigation Costs

Options	Holding North Right-of-way Line			Holding South Right-of-way Line		
	Estimated Wetland/Surface Water Impact Acres	Estimated Mitigation Cost	Total Cost w/ Permit Fee	Estimated Wetland/Surface Water Impact Acres	Estimated Mitigation Cost	Total Cost w/ Permit Fee
Option 1A	7.31	\$343,777	\$349,400	7.31	\$343,343	\$349,400
Option 1B	7.31	\$343,777	\$349,400	7.31	\$343,343	\$349,400
Option 2	7.39	\$347,348	\$353,000	7.33	\$345,520	\$353,000
Option 3	7.39	\$347,348	\$353,000	7.33	\$345,520	\$353,000

Social and Economic



Socio-economic data for Osceola County was pulled using the 2015-2019 Census Tract data. The data includes general population data, demographics, environmental justice data, limited English abilities, and access to personal vehicles. The data also noted 13% of Osceola County's population lives below the poverty line. The median household income is \$52,279 for the area. Access to multi-modal transportation can benefit impoverished communities that may not have access to a vehicle or other means of transportation.

This project will provide improved multi-modal transportation options for the surrounding community. Improving the pedestrian and bicycle connectivity will especially enhance the quality of life for households which do not have access to a vehicle. Each option will create additional opportunity for the community to access schools, supermarkets, churches, recreational facilities, restaurants, and other businesses. The enhanced multi-modal access opportunity will promote the economic and social health of the surrounding area.

The vehicular access from Partin Settlement Road to some properties may be affected by the proposed median. The entrance to Restore the Path for Christ Church will become a right-in and right-out only driveway. The Life Church entrance will also become a right-in and right-out only driveway, but the access to Aeronautical Drive will remain as-is. Multi-modal access to all churches along this portion of Partin Settlement Road will be enhanced with the implementation of sidewalks as part of each option for this project.

Cultural Resources

A Cultural Resource Assessment Survey (CRAS) was conducted by the project team and a summary of the findings from the report are discussed below.

The CRAS included an archeological survey and architectural history survey within the project area. The survey area extends approximately 250 feet from the centerline of Partin Settlement Road to include the existing and proposed right-of-way for the project along with proposed pond sites. The survey area was expanded beyond these limits in certain areas where preliminary pond sites have been proposed.

Shovel tests were performed within the survey area in September 2020, which completed testing of the archaeological survey area. Much of the landscape reflects modification that occurred during the twenty-first century, including recent disturbances associated with new development and utility work.

Three previously recorded archaeological sites (8OS00125, 8OS00126, 8OS01844) located within the survey area were not identified during the current investigation and appear to have been destroyed by development. All have been previously determined as not eligible for listing in the National Register of Historic Places (NRHP).

Two isolated finds, FS-1 and FS-3, were identified in a pasture at the proposed site for Pond 3A(1) in the northwest quadrant of Cross Prairie Parkway and Neptune Road at the western end of the project area. Isolated finds by their definition are limited in their potential research value.

Therefore, these findings are not eligible to the NRHP and no further work is recommended. One indeterminate prehistoric archaeological site (8OS03098) was identified at the same pond site. The site is a sparse, low-density lithic scatter and does not contain intact subsurface cultural features or any other primary cultural deposits. This site has no potential to provide further important information beyond what has already been documented. This site is recommended as not eligible for listing on the NRHP under any of the four criteria. No further work is recommended for this site.

No other artifacts were recovered within the survey area, and no additional archaeological sites or occurrences were identified during the survey. The project team recommends that no further archaeological investigation is warranted for the project as it is currently designed. The CRAS has been reviewed by the Florida Division of Historical Resources which found the submitted report to be complete and sufficient in accordance with Chapter 1A-46, *Florida Administrative Code*. A copy of this correspondence is in Appendix A.

Physical Resources

Contamination Screening Evaluation Report

A Level 1 Contamination Screening Evaluation Report (CSER) was performed on December 18, 2020 in general accordance with the FDOT *Project Development and Environment (PD&E) Manual* dated January 14, 2019. The objective of this study is to evaluate the potential for contamination that may affect various alignment alternatives as well as future construction activities along the roadway corridor. The CSER indicates that further assessment may be required for Medium ranked sites to verify or determine the extent of contamination concerns depending on the location of the proposed improvements.

The CSER identified the following Medium Risk sites that may be affected by one or more of the typical section options:

- Chevron Gas Station at Neptune Road and Partin Settlement Road – this site will affect all typical section alternatives.
- Johnston Property, 2575 Partin Settlement Road – Depending on the siting of Pond 5(2), this site may be adjusted to a Low Risk site, though it would apply to all typical section options.
- Jimmy Hickman Excavating - Depending on the siting of Pond 5(2), this site may be adjusted to a Low Risk site, though it would apply to all typical section options.
- Pond 8A(1) – This pond abuts the Partin Settlement Road south right-of-way line and would be affected only by those alternatives where right-of-way takes are needed to the south of the current Partin Settlement right-of-way.

For more information, please see the Level 1 Contamination Screening Evaluation Report.

Engineering Evaluation

Proposed Pond Sites

The construction of the proposed roadway improvements will introduce additional impervious areas and therefore an increase in stormwater discharge would be anticipated. The ponds will be designed using wet



detention pond design criteria due to the high ground water table in the area and will provide for water quality treatment and water quantity attenuation. The stormwater ponds have been designed and sized to accommodate the proposed roadway typical sections. The stormwater management facilities for the project were designed in compliance with the guidelines set forth by FDOT, SFWMD and Osceola County. Stormwater treatment, attenuation, and nutrient reduction will comply with the guidelines as defined in the FDOT Drainage Manual, Chapter 40D-4 of the F.A.C. and the SFWMD ERP manual.

The roadway project limits have been subdivided into eight drainage basins (see attached drainage maps on the next three pages). The water quality and attenuation for Basins 1, 2, 4, and 7 will be provided for existing wet detention ponds which already serve part of the Partin Settlement Road corridor. Basin 6 will be provided for future Turnpike ponds, and Basins 3A, 3B, 5, and 8 will be provided for in proposed pond sites or as expansions of existing ponds. For all basins that will require the construction of a new pond site or the expansion of an existing pond, at least two alternatives were evaluated. The pond sites were evaluated using a volumetric analysis, which accounts for water quality treatment and water quantity attenuation for runoff. The volumetric analyses of the pond sites were performed with preliminary data, reasonable engineering judgement, and assumptions.

In the following pages, there is discussion about eight basins that were researched/analyzed for this study. Where proposed pond sites or expansions of existing ponds are located, a table was provided with additional information about the proposed sites. In these tables, the right-of-way costs are based on Unit Prices for Pond Sites as Prepared By Barfield Group. For Pond 8A(2), the right-of-way cost is based on a take of the entire parcel and all other right-of-way costs based on a take equal to the estimated right-of-way take size.

The wetland mitigation cost for Pond 3B(2) is assumed to be zero due to the wetlands encountered in the upland area having trapped water. However, the wetland mitigation costs are estimated at \$48,720 if mitigation is required due to impacts for Pond 3B(2). The wetland mitigation cost could be reduced to \$0 if the existing wetlands on the Pond 8A(2) parcel do not require mitigation. These wetlands were previously filled by possible unpermitted activity.

The proposed Partin Settlement Road improvements will be divided into two phases with the section from Neptune Road to east of Remington Boulevard being constructed first and the remaining section to Lakeshore Boulevard being constructed later depending on available funding. For the first phase, Basin 8A assumes that widening and transitions will end near Simmons Road which will be served by Pond 8A (1). For the second phase, the widening will end at Lakeshore Boulevard and will be served by Pond 8B(2).

Basin 1

The preferred pond site for Basin 1 is existing Pond 300 from SFWMD Permit No. 49-01518-P (Neptune Road Widening Project) which serves Neptune Road and the western end of Partin Settlement Road. No modifications to existing Pond 300 are required in the proposed condition. The pre/post-development amount of impervious area and the flow rate to the existing Neptune Road storm sewer system would need to be balanced in the proposed condition. See Figure 16 for basin locations.



Basin 2

The preferred pond site for Basin 2 is existing Pond 1 from SFWMD Permit No. 49-103652-P (Shady Lane Extension Project, also known as Cross Prairie Parkway) which serves a segment of Partin Settlement Road at its intersection with Cross Prairie Parkway. No modifications to existing Pond 1 are required in the proposed condition to serve Basin 2. The pre/post-development amount of impervious area and the flow rate to the existing Cross Prairie Parkway storm sewer system would need to be balanced in the proposed condition. See Figure 16 for basin locations.

Basin 3A

Two potential pond sites, 3A(1) and 3A(2), were evaluated for Basin 3A. The preferred pond site for Basin 3A is Pond 3A(1). Pond 3A(1) would expand the existing Pond 1 from Cross Prairie Parkway (see Basin 2) and also be jointly used by the Neptune Road widening project which is being designed concurrently with the Partin Settlement Road widening project. The pond site location would be a partial parcel acquisition under private ownership (Divirgillio Family Partnership LTD).

The preliminary pond sizing calculations indicate that Basin 3A requires a footprint of 4.57 acres if there are impacts to offsite stormwater management (SWM) systems and a footprint of 2.73 acres if there are no impacts to offsite SWM systems. The Pond 3A(1) site has only 3.0 acres of space available on the parcel for the proposed pond, therefore, Pond 3A(1) is only an option if there will be no impacts to offsite SWM systems in Basin 3A. Pond 3A(1) will also be located approximately 5,050 feet away from the low spot on Partin Settlement Road which will require larger than standard pipes to convey the runoff to the pond due to the HGL losses in the storm sewer system.

Pond 3A(2) would be a newly constructed pond and would be located on the south side Partin Settlement Road and just west of cross drain ECD-1. The pond site location would be a partial acquisition of a parcel under private ownership (Divirgillio Family Partnership LTD). The site has wetland and floodplain concerns and there is a pre-existing, ongoing development plan for the site by the current owner. See Figures 16 and 18 for basin locations.

Table 12: Basin 3A Pond Sites Summary

Factor	Basin 3A Pond Sites			
	Impacts to Existing Offsite SWM		No Impacts to Existing Offsite SWM	
	3A(1)	3A(2)	3A(1)	3A(2)
Pond Size	N/A	2.73	2.72	1.69
No. of Property Owners	1	1	1	1
Land Use	Pastureland	Pastureland	Pastureland	Pastureland
Section 4(f) Property	No	No	No	No
Floodplains Impacts	No	No	No	No
Wetland Impacts	No	Yes	No	Yes
Wildlife Impacts	No	No	No	No
Cultural/Archeological Impacts	Low	No	No	No
Utilities	No	No	No	No
Contamination	No	No	No	No
Public Welfield	No	No	No	No
Joint Use Pond	Yes	No	Yes	No
Raised Roadway Elevation Required	No	No	No	No
Wetland Mitigation Cost	N/A	\$24,160	\$0	\$34,160
Pond Construction Cost	N/A	\$799,713	\$729,007	\$494,944
Right-of-way Cost	N/A	\$467,350	\$1,063,900	\$467,350
Storm Sewer to Pond from Road R/W	N/A	\$42,524	\$332,917	\$42,524
Storm Sewer from Pond to Discharge Point	N/A	\$24,235	\$0	\$24,235
Total Cost	N/A	\$1,367,982	\$2,125,824	\$1,063,213
Recommendation/Ranking	2	1	1	2

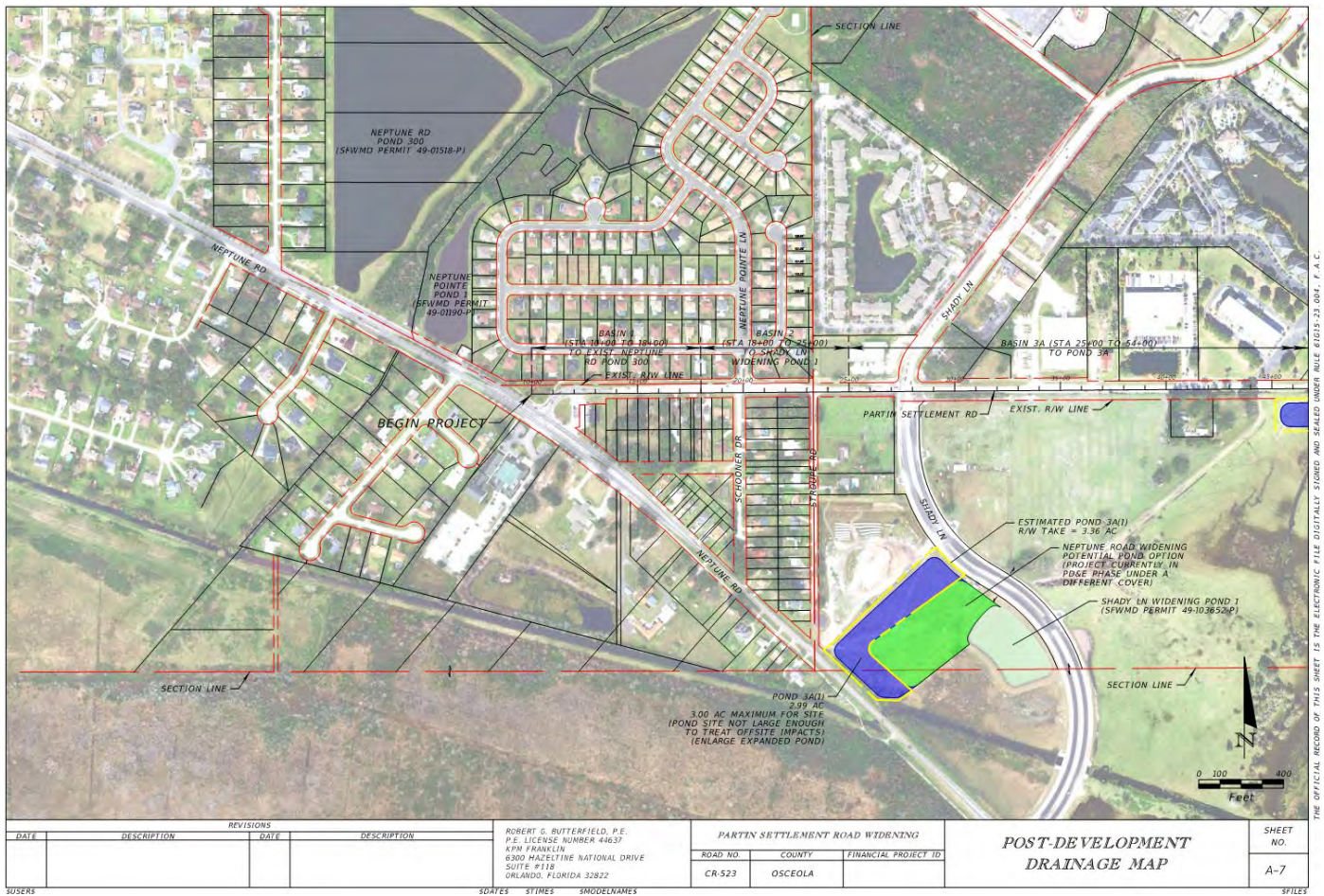


Figure 16: Drainage Map (Basin 1 to Basin 3A)

Basin 3B

Two potential pond sites, 3B(1) and 3B(2) were evaluated for Basin 3B. The preferred pond site for Basin 3B is Pond 3B(2) due to its lower cost estimate and the increased design flexibility allowed from modifying a pond already owned by the County. Pond site location 3B(1) would be a partial parcel acquisition under private ownership (Dirt IV). Adjacent to this parcel to the south is an existing wet detention pond which serves the Partin Settlement Promenade and is under private ownership (Echo Partin Village LLC). Pond option 3B(1) would expand Pond A from SFWMD Permit No. 49-02142-P (Partin Settlement Promenade). The parcel owned by Dirt IV is 5.50 acres in area and the proposed Pond 3B(1) footprint is 0.90 acres. The existing pond site is adjacent to the north side of Partin Settlement Road and discharges to the north end of ECD-1. The pond site would have wetland impacts and minimal floodplain impacts.

Pond site location 3B(2) is a partial parcel acquisition under private ownership (Divrigilio Family Partnership LTD). Adjacent to the pond site location to the west is an existing wet detention pond which serves Cobblestone Apartments. Pond option 3B(2) would expand Pond 1 from SFWMD Permit No. 49-01072-P (Cobblestone). According to documents in the SFWMD permit for Cobblestone, the property had been

intended to serve single family homes. The parcel is 42.28 acres in areas and the proposed Pond 3B(2) footprint is 0.72 acres. Adjacent to this parcel to the west is an existing wet detention pond which is owned by Osceola County and which primarily serves the Cobblestone Apartments. It is proposed that Pond 3B(2) will be an expansion of the existing Osceola County pond. The proposed expansion width of the pond is expected to allow the parcel remainder to continue to be developed as single-family homes with shorter parcel lengths. The existing wet detention pond is adjacent to the south side of Partin Settlement Road and just east of cross drain ECD-1. Pond 3B(2) will have wetland impacts, however, the wetland impacts might not require mitigation since the wetlands are in areas previously permitted as uplands. See Figure 18 for basin locations.

Table 13: Basin 3B Pond Sites Summary

Factor	Basin 3B Pond Sites	
	3B(1)	3B(2)
Pond Size	0.9	0.72
No. of Property Owners	1	1
Land Use	Commercial	Pastureland
Section 4(f) Property	No	No
Floodplains Impacts	Minimal	No
Wetland Impacts	Yes	Yes
Wildlife Impacts	No	No
Cultural/Archeological Impacts	No	No
Utilities	No	No
Contamination	No	No
Public Wellfield	No	No
Joint Use Pond	Yes	Yes
Raised Roadway Elevation Required	No	No
Wetland Mitigation Cost	\$36,000	\$0
Pond Construction Cost	\$81,544	\$64,933
Right-of-way Cost	\$963,400	\$485,800
Storm Sewer to Pond from Road R/W	\$26,973	\$26,973
Storm Sewer from Pond to Discharge Point	\$0	\$0
Total Cost	\$1,107,918	\$577,707
Recommendation/Ranking	2	1



Basin 4

The preferred pond site for Basin 4 is existing Pond 1 from SFWMD Permit No. 88-00028-S (US 192 from Aeronautical Boulevard to Budinger Avenue) which serves a short segment of Partin Settlement Road at the intersection with US 192. No modifications to existing Pond 1 are required in the proposed condition. The pre/post-development amount of impervious area and the flow rate to the existing US 192 storm sewer system would need to be balanced in the proposed condition. See Figure 18 for basin locations.

Basin 5

Three potential pond sites, 5(1), 5(2), and 5(3) were evaluated for Basin 5. The preferred pond site for Basin 5 is Pond 5(2). Pond 5(1) is no longer an option because the parcel is to be developed by St Cloud Commercial Properties, LLC. Pond 5(2) is the preferred pond site because there is a residential structure and other on-site facilities within the Pond 5(3) site which would have to be taken for Pond 5(3) to be viable and the estimated cost for Pond 5(2) is within 5% of the estimated cost for Pond 5(3).

Pond site location 5(1) is a partial parcel acquisition under private ownership (St Cloud Commercial Properties LLC). The parcel size is 2.12 acres and the proposed Pond 5(1) footprint is 0.91 acres. The proposed pond site is adjacent to the north side of Partin Settlement Road. There is a potential low contamination risk for the pond site due to possible contamination from the adjacent property to the east. The potential pond site has been previously permitted through SFWMD Permit No. 49-100255-P in 2018 for development by St Cloud Commercial Properties, LLC.

Pond site location 5(2) is a partial parcel acquisition under private ownership (Gateway Baptist Church, Inc.). The parcel size is 4.55 acres and there is approximately 2.10 acres on the site that has not been developed. The proposed pond footprint is 1.18 acres if the treatment and attenuation of the existing church is not included within Pond 5(2). The footprint is 1.43 acres if the treatment and attenuation of the existing church is included, however, the elevation of Partin Settlement Road would also need to be raised approximately 0.8 ft in Basin 5 for the pond to function within this footprint. There is a potential low contamination risk for the pond site due to possible contamination from the adjacent property to the west. A 60-foot wide drainage easement will be needed to access Pond 5(2) to allow for both inflowing and outflowing pipes.

Pond site location 5(3) is a partial parcel acquisition under private ownership (David and Heather Johnston). The parcel size is 2.14 acres, and the proposed Pond 5(3) footprint is 0.87 acres. The proposed pond is adjacent to the north side of Partin Settlement Road. There is a potential low contamination risk for the pond site due to possible contamination from the adjacent property. There is a residential structure on the parcel which would have to be taken as part of the right-of-way acquisition for Pond 5(3) to be a viable option. The preliminary pond design allows for a 20-foot wide access strip on the east side of the parcel to allow access to the remainder of the parcel. See Figure 18 for basin locations.

Table 14: Basin 5 Pond Sites Summary

Factor	Basin 5 Pond Sites			
	5(1)	5(2)		5(3)
		Impacts to Church Not Included	Impacts to Church Included	
Pond Size	N/A	2.73	2.72	1.69
No. of Property Owners	1	1		1
Land Use	Commercial	Churches		Commercial
Section 4(f) Property	No	No		No
Floodplains Impacts	No	No		No
Wetland Impacts	No	No		No
Wildlife Impacts	No	No		No
Cultural/Archeological Impacts	No	No		No
Utilities	No	No		No
Contamination	Low	Low		Low
Public Wellfield	No	No		No
Joint Use Pond	No	No		No
Raised Roadway Elevation Required	No	No	Yes	No
Wetland Mitigation Cost	\$0	\$0	\$0	\$0
Pond Construction Cost	\$205,317	\$268,399	\$324,667	\$196,310
Right-of-way Cost	\$1,902,270	\$1,951,550	\$1,951,550	\$2,036,525
Storm Sewer to Pond from Road R/W	\$20,049	\$73,801	\$88,207	\$17,496
Storm Sewer from Pond to Discharge Point	\$49,424	\$123,369	\$123,369	\$68,086
Total Cost	\$2,177,060	\$2,417,119	\$2,487,793	\$2,318,417
Recommendation/Ranking	3	1	1	2

Basin 6

The preferred pond sites for Basin 6 are the future Florida Turnpike's ponds that will be serving that basin. The Turnpike is currently designing the Florida's Turnpike (SR 91) widening with express lanes from south of SR 500 (US 192) to Osceola Parkway (FPID No. 436194-1-52-01). The consultant drainage engineer for the Turnpike has indicated that their project will collect, convey, and treat the runoff from areas from Partin Settlement Road that are currently draining into the Turnpike drainage system. See Figure 18 for basin locations.

Basin 7

The preferred pond site for Basin 7 is existing Pond 25 from SFWMD Permit No. 9-00544-S (Remington Boulevard Extension and Recreation Facility Project) serving Remington Boulevard and a portion of Partin Settlement Road.



Figure 17: Existing Stormwater Management Pond

No modifications to existing Pond 25 are required in the proposed condition. The pre/post-development amount of impervious area and flow rate to the existing Remington Boulevard storm sewer system would need to be balanced in the proposed condition. See Figures 18 and 19 for basin locations.

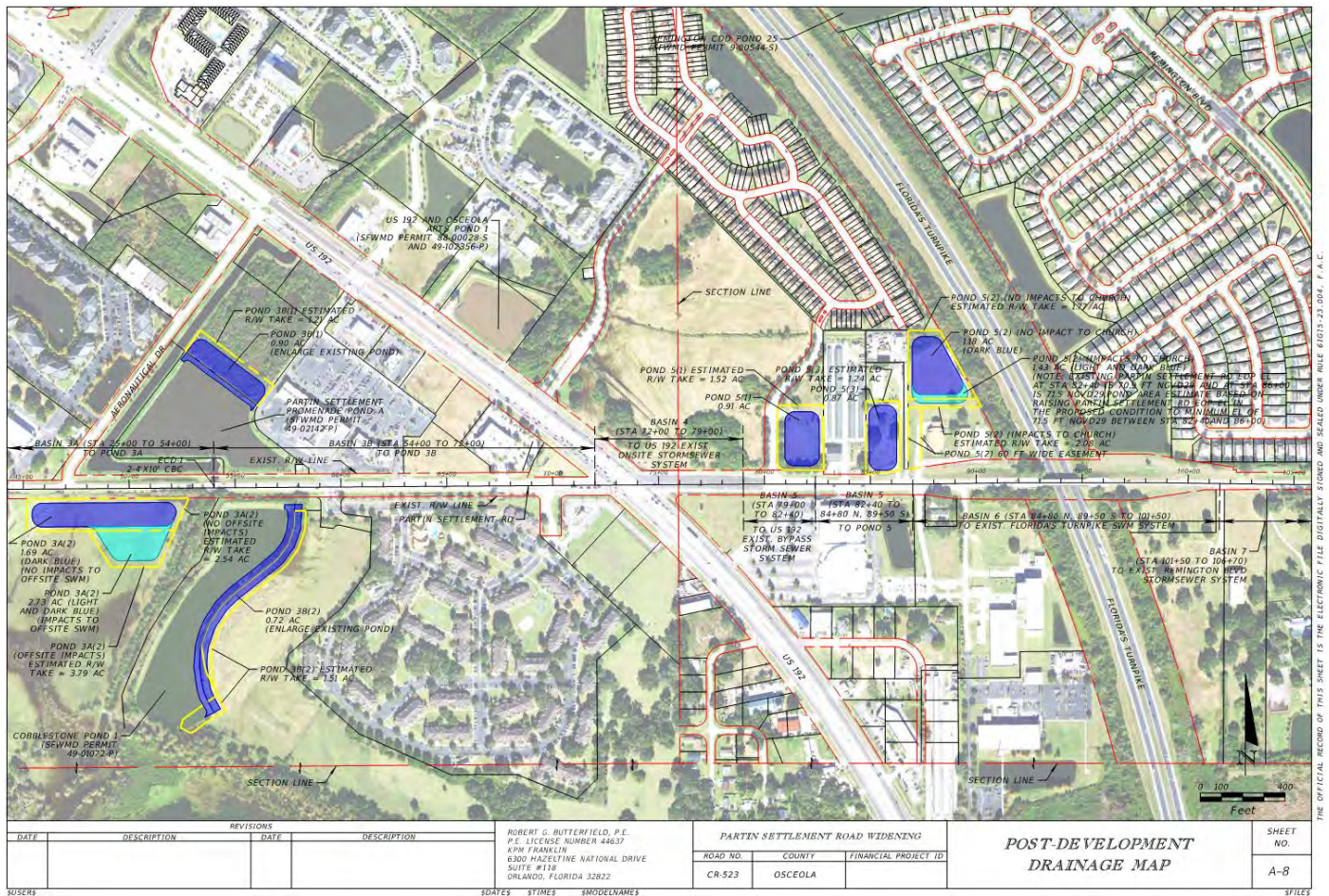


Figure 18: Drainage Map (Basin 3A Cont. to Basin 7)

Basin 8A

Basin 8 has several options depending on where the eastern limits of the roadway project will terminate. Basin 8A assumed the project terminates in the vicinity of Simmons Road. The preferred pond site for Basin 8A is Pond 8A(1) due to its lower cost estimate. Basin 8B assumes the project will extend all the way to Lakeshore Boulevard and that the 8B ponds would treat and attenuate the entirety of Basin 8.

The preferred pond site for Basin 8B is Pond 8B(2) due to its lower cost estimate and its better engineering feasibility. The better feasibility is caused from the site being located three (3) feet lower in elevation, and because Partin Settlement Road would need to be raised in the proposed condition if Pond 8B(1) site location is chosen. The preliminary calculations for Pond 8B assume that Pond 8A will not be built, therefore, Pond 8B is designed to treat and attenuate the entirety of Basin 8 from the western limit of Basin 8 to Lakeshore Boulevard.

Pond site location 8A(1) is a partial parcel acquisition under private ownership (Ramdan Dave, Ramdan Saraswattie). The parcel size is 11.67 acres and the proposed Pond 8A(1) footprint is 1.70 acres. The

proposed pond site is adjacent to the south side of Partin Settlement Road. The site will have wetland impacts and there is a potential low contamination risk due to previous farming activities.

Pond site location 8A(2) requires the acquisition of two parcels under private ownership (Syed I Zaidi & Munawar S Zaidi Rev Trust). The two parcel sizes are 1.1 acres and 1.3 acres in area and the proposed Pond 8A(2) footprint is 1.71 acres. The proposed pond site is adjacent to the south side of Partin Settlement Rd. The site will have wetland impacts and there is a potential low contamination risk for the pond site due to previous farming activities. See Figure 19 for basin locations.

Table 15: Basin 8A Pond Sites Summary

Factor	Basin 8A Pond Sites	
	8A(1)	8A(2)
Pond Size	1.70	1.7
No. of Property Owners	1	2
Land Use	Residential	Vacant
Section 4(f) Property	No	No
Floodplains Impacts	No	No
Wetland Impacts	No	Yes
Wildlife Impacts	No	No
Cultural/Archeological Impacts	No	No
Utilities	No	No
Contamination	Low	Low
Public Wellfield	No	No
Joint Use Pond	No	No
Raised Roadway Elevation Required	No	No
Wetland Mitigation Cost	\$0	\$63,947
Pond Construction Cost	\$497,383	\$499,821
Right-of-way Cost	\$594,985	\$863,600
Storm Sewer to Pond from Road R/W	\$15,736	\$75,438
Storm Sewer from Pond to Discharge Point	\$36,982	\$36,982
Total Cost	\$1,145,086	\$1,539,789
Recommendation/Ranking	1	2

Basin 8B

Pond site location 8B(1) is a partial parcel acquisition under private ownership (3044 Partin Realty Trust Bezema Erik). The parcel size is 4.87 acres and the proposed Pond 8B(1) footprint is 3.13 acres. Note that the surface area calculated for Pond 8B(1) requires that the elevation of Partin Settlement Road be raised approximately two feet in the vicinity of the pond site, therefore, the pond site is not a feasible option.

Pond site location 8B(2) is a partial parcel acquisition under private ownership (3056 Partin Realty Trust Bezema Eric TR). The parcel size is 4.94 acres and the proposed Pond 8B(2) footprint is 2.56 acres. The proposed pond site is adjacent to the south side of Partin Settlement Road. This pond site lies about three feet lower in elevation than Pond 8B(1) and thereby does not require raising of the roadway to fully treat the project limits. Pond site location 8B(2) has potential wetland impacts. See Figure 19 for basin locations.

Table 16: Basin 8B Pond Sites Summary

Factor	Basin 8B Pond Sites	
	8B(1)	8B(2)
Pond Size	3.13	2.56
No. of Property Owners	1	1
Land Use	Vacant	Residential
Section 4(f) Property	No	No
Floodplains Impacts	No	No
Wetland Impacts	Yes	Yes
Wildlife Impacts	No	No
Cultural/Archeological Impacts	No	No
Utilities	No	No
Contamination	No	No
Public Welfield	No	No
Joint Use Pond	No	No
Raised Roadway Elevation Required	Yes	No
Wetland Mitigation Cost	\$224,000	\$226,400
Pond Construction Cost	\$916,744	\$748,512
Right-of-way Cost	\$1,576,365	\$1,596,660
Storm Sewer to Pond from Road R/W	\$25,953	\$25,953
Storm Sewer from Pond to Discharge Point	\$128,225	\$83,534
Total Cost	\$2,871,288	\$2,681,059
Recommendation/Ranking	2	1

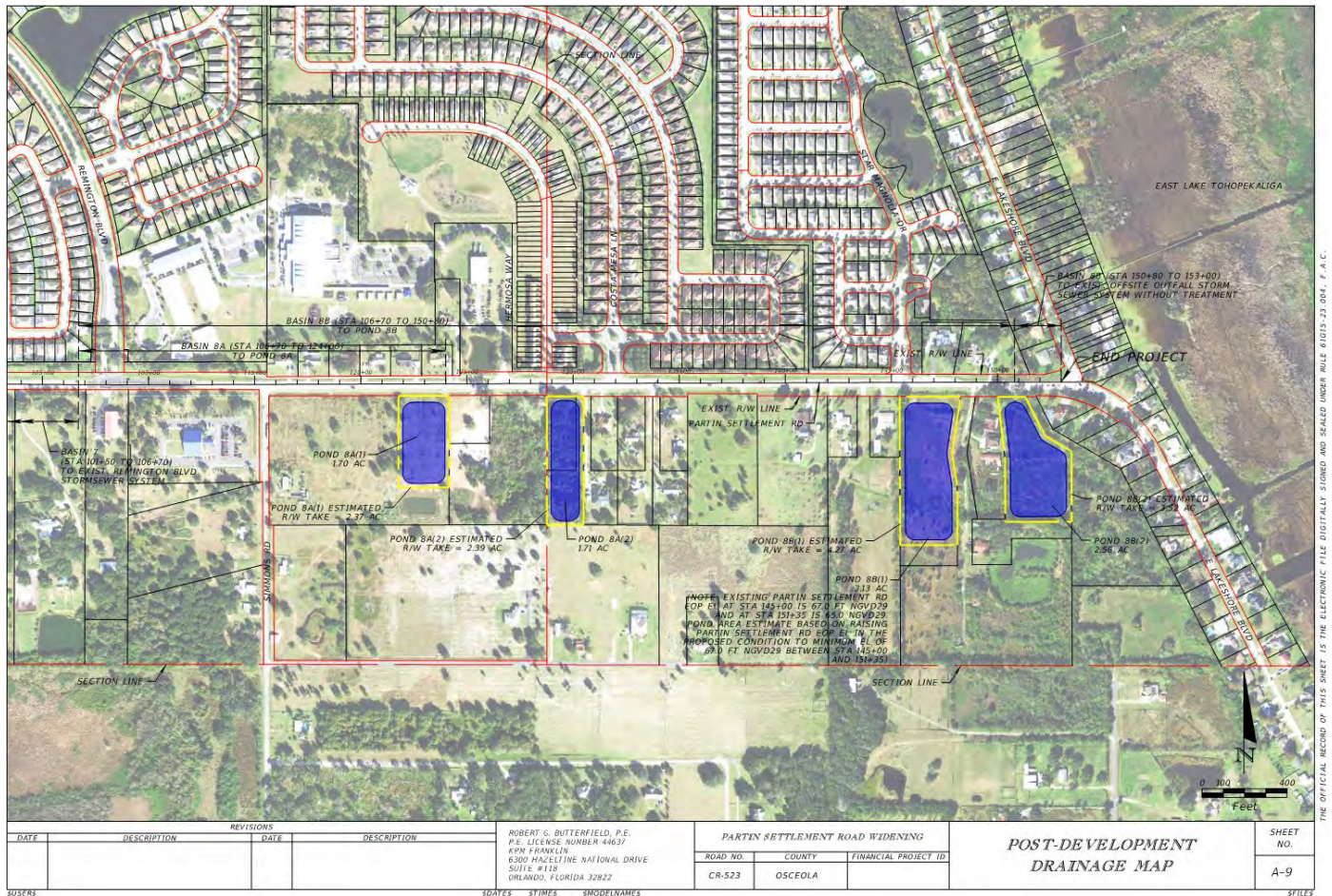


Figure 19: Drainage Map (Basin 7 Cont. to Basin 8B)

Utilities

The project team reached out to 14 local utility companies to provide approximate mapping and information about existing underground utilities along the roadway. Most of the existing utilities are located within the project right-of-way, though Florida Gas Transmission (FGT) has a line crossing the east approach of the Partin Settlement Road/US 192 intersection where any additional roadway widening and associated right-of-way acquisition would likely impact FGT's easement. Also, FGT has specific requirements related to the design and construction of the roadway and storm sewer system which crosses their lines.

Each typical section option is likely to equally affect those existing utilities located within the project right-of-way. The preliminary utility information is shown on the right-of-way maps in Appendix B.



Construction Cost

Cost estimates for options 1A, 1B, 2 and 3 have been developed based on the conceptual design. These estimates are summarized in Table 17 below. Earthwork, roadway pavement, edge of pavement to right-of-way maintenance, median construction, drainage, intersections, signage, lighting, signalization and interconnectivity/ITS and landscaping costs have all been considered and included in the overall option costs below.

Table 17: Estimated Construction Costs Summary

Total Estimated Construction and CEI Costs		
Option	Hold North Right-of-way Line	Hold South Right-of-way Line
Option 1A	\$37,282,718	\$37,282,718
Option 1B	\$37,803,200	\$37,803,200
Option 2	\$38,122,532	\$38,122,532
Option 3	\$37,544,435	\$37,544,435

Right-of-way Needs

The right-of-way impacts for each option were broken out and evaluated for all estimated parcel acquisitions. A summary of the factored right-of-way acquisition areas and costs are found in table 18 below. All estimated parcel acquisition values were found using the total land area and estimated total cost for the Preferred Ponds 3A(1), 3B(2), 5(2), and 8(B)2.

Table 18: Estimated Right of Way Costs Summary

Total Estimated Right-of-way Costs				
Option	Hold North Right-of-way Line		Hold South Right-of-way Line	
	Right-of-way Impacts (acres)	Right-of-way Cost	Right-of-way Impacts (acres)	Right-of-way Cost
Option 1A	12.145	\$8,534,300	12.473	\$10,566,625
Option 1B	12.145	\$8,534,300	12.473	\$10,566,625
Option 2	13.408	\$11,801,360	13.713	\$14,588,270
Option 3	13.408	\$11,801,360	13.713	\$14,588,270



Project Costs

The combined construction, right-of-way and environmental mitigation costs for each alternative are summarized in table 19 below.

Table 19: Estimated Alternatives Cost Summary

Total Alternative Costs				
Alternative	Construction Costs	Right-of-way Costs	Environmental Costs w/ Permit Fee	Total Alternative Cost
Option 1A–Hold North Right-of-way	\$37,282,718	\$8,534,300	\$349,400	\$46,166,418
Option 1A–Hold South Right-of-way	\$37,282,718	\$10,566,625	\$349,400	\$48,198,743
Option 1B–Hold North Right-of-way	\$37,803,200	\$8,534,300	\$349,400	\$46,686,900
Option 1B–Hold South Right-of-way	\$37,803,200	\$10,566,625	\$349,400	\$48,719,225
Option 2–Hold North Right-of-way	\$38,122,532	\$11,801,360	\$353,000	\$50,276,892
Option 2–Hold South Right-of-way	\$38,122,532	\$14,588,270	\$353,000	\$53,063,802
Option 3–Hold North Right-of-way	\$37,544,435	\$11,801,360	\$353,000	\$49,698,795
Option 3–Hold South Right-of-way	\$37,544,435	\$14,588,270	\$353,000	\$52,485,705

Table 20: Partin Settlement Road Alternatives Evaluation Matrix

EVALUATION MATRIX										
PARTIN SETTLEMENT ROAD WIDENING/RECONSTRUCTION										
NEPTUNE ROAD to LAKESHORE BOULEVARD										
OSCEOLA COUNTY										
	UNITS	No Build Option	OPTION 1A		OPTION 1B		OPTION 2		OPTION 3	
			NORTH Alignment	SOUTH Alignment	NORTH Alignment	SOUTH Alignment	NORTH Alignment	SOUTH Alignment	NORTH Alignment	SOUTH Alignment
Right-of-Way Needs										
Parcel Impacts (Residential + Business = Total)	Each	0	12 + 18 = 30	17 + 22 = 39	12 + 18 = 30	17 + 22 = 39	14 + 22 = 36	17 + 22 = 39	14 + 22 = 36	17 + 22 = 39
Potential Relocations (Residential + Business = Total)	Each	0	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1	1 + 0 = 1
Community Facilities Impacted*	Each	0	5	3	5	3	5	3	5	3
Right-of-Way Acreage Impacts	Acres	0	12.15	12.47	12.15	12.47	13.41	13.71	13.41	13.71
Environmental Impacts										
Anticipated Wildlife / Habitat Impacted	Yes/No	No	No	No	No	No	No	No	No	No
Bald Eagle Nest	Each	0	0	0	0	0	0	0	0	0
Floodplains	Acres	0.00	1.90	1.98	1.90	1.98	2.35	2.23	2.35	2.23
Wetland and Surface Water Impacts	Acres	0.00	5.63	5.63	5.63	5.63	5.67	5.67	5.64	5.64
Potential Contamination Parcels (Low + Medium + High = Total)	Each	0 + 1 + 0 = 1	2 + 2 + 0 = 4	5 + 1 + 0 = 6	2 + 2 + 0 = 4	5 + 1 + 0 = 6	2 + 2 + 0 = 4	5 + 1 + 0 = 6	2 + 2 + 0 = 4	5 + 1 + 0 = 6
Traffic Projections										
Volume/Capacity (V/C) Ratio (Design Year 2045)	V/C	1.19 - 2.14	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98	0.65 - 0.98
Preliminary Cost Estimates										
Preliminary Right-of-Way Costs - Neptune Road to Remington Blvd	\$	\$0	\$ 6,673,495	\$ 7,541,010	\$ 6,673,495	\$ 7,541,010	\$ 9,330,305	\$ 11,159,395	\$ 9,330,305	\$ 11,159,395
Preliminary Right-of-Way Costs - Remington Blvd to Lakeshore Boulevard	\$	\$0	\$ 1,860,805	\$ 3,025,615	\$ 1,860,805	\$ 3,025,615	\$ 2,471,055	\$ 3,428,875	\$ 2,471,055	\$ 3,428,875
Preliminary Construction and CEI Costs - Neptune Road to Remington Blvd	\$	\$0	\$ 27,092,318	\$ 27,092,318	\$ 27,423,406	\$ 27,423,406	\$ 27,704,824	\$ 27,704,824	\$ 27,270,753	\$ 27,270,753
Preliminary Construction and CEI Costs - Remington Blvd to Lakeshore Blvd	\$	\$0	\$ 10,190,400	\$ 10,190,400	\$ 10,379,793	\$ 10,379,793	\$ 10,417,709	\$ 10,417,709	\$ 10,273,682	\$ 10,273,682
Preliminary Environmental Mitigation Cost	\$	\$0	\$ 349,400	\$ 349,400	\$ 349,400	\$ 349,400	\$ 353,000	\$ 353,000	\$ 353,000	\$ 353,000
Total Preliminary Estimated Project Cost Estimate**	\$	\$0	\$46,166,418	\$48,198,743	\$46,686,900	\$48,719,225	\$50,276,892	\$53,063,802	\$49,698,795	\$52,485,705
LEGEND LITTLE/ NO IMPACTS MEDIUM IMPACTS HIGH IMPACTS										
<p>*The community facilities impacted are churches within the corridor. These church impacts are accounted for in the total parcels impacted as a business. **This project may be phased based on final traffic analysis results. The costs shown reflect the total preliminary estimated projects costs from Neptune Road to Lakeshore Boulevard.</p>										

VI. RESULTS OF THE PUBLIC INVOLVEMENT ACTIVITIES

Public Involvement

Osceola County held both virtual and in-person public information meetings regarding the Partin Settlement Road Improvements on January 21, 2021. The public meetings provided an opportunity for the public to learn more about the alternatives developed by County staff which have been designed to address growing traffic demands in the corridor, while providing multi-modal accommodations along with safety enhancements. The proposed project improvements include four travel lanes, a raised median, sidewalks for bicyclists and pedestrians, drainage improvements, and traffic signal modifications.

Public Notification

Meeting invitations were sent by email to 29 elected officials and 61 appointed officials, agency representatives, and Native American Tribes. Additional notifications were also sent by U.S. Mail to 1,525 property owners and tenants along the project corridor. The invitations included the date and time of the public meeting, the three options to attend the meeting (virtually, in person, by phone), and directions to register for the meeting.

Meeting Advertisement

The meetings were advertised with a newspaper display ad published in the *Osceola News Gazette* on Thursday, January 14, 2021. The meeting notice was also posted on the Osceola County website. Osceola County also distributed a news release and posted the meeting information on social media. Copies of the newspaper display ad and press release are included in Appendix B.

Public Meeting Format

The public meetings were held both virtually and in person. The virtual meeting was held online using GoToWebinar. The webinar opened at 6 p.m. and included a presentation with an overview of the project.

Following the presentation, a question-and-answer session was held to allow project team members to address comments submitted by attendees through the Question pane of GoToWebinar. The virtual meeting ended at 7:30 p.m.



The in-person meeting was held at the Council on Aging, 700 Generation Point, Kissimmee, FL 34744 with doors opened at 5:30 p.m. There were seven exhibit boards set up with social-distance spacing. Several handouts were provided at the meeting. Attendees could view the exhibits, ask team members questions, and were also able to view a looping video presentation with the same information as the virtual meeting presentation.



Meeting Attendance

Twenty-seven people signed into the virtual meeting and 14 people attended the in-person meeting. Attendees also included project team members, consultants, FDOT representatives, a representative with Osceola County Schools, and City of St. Cloud employees.

Handouts

A copy of the presentation and a comment form were available for download during the virtual meeting. Participants were encouraged to ask questions and/or leave comments in the comment box provided. They were also told they could email the comments or go to

the comment form and fill it out. In-person attendees picked up a folder on their way into the meeting which contained a copy of the presentation, a comment form, sign card, project flyer and a pen.

Public Comments

A total of 25 questions and/or comments were submitted during the virtual meeting, two comment forms were received during the in-person meeting and several community members asked questions or provided comments by email to the project manager before and after the meeting. Comments were generally neutral or positive regarding the need for the project. Questions submitted generally concerned the following topics:

- Property acquisition needs
- Access to properties
- Speed limits
- Drainage improvements
- Lighting
- Safety of bicycle lanes on the street vs. multi-use pathways
- Location of turn lanes
- Construction start date and duration





- Florida's Turnpike's planned improvements
- Potential impacts to wildlife
- Landscaping/median landscaping

A summary of questions and responses to public inquiries during the course of the study are included in Appendix C of this report.



VII. RECOMMENDED PREFERRED ALTERNATIVE

Recommended Preferred Alternative

Based on the responses from the public information meetings as well as evaluation information developed during the study, County staff and their consultant team recommended the preferred alternative to consist of Typical Section 1A using the North Alignment. The estimated construction and right-of-way costs are amongst the lowest for this alternative, and this option has the fewest parcel impacts.

Typical Section 1A also provides all of the necessary project features to meet future traffic demands as well as multi-modal accommodations including added lanes, raised median, sidewalks for bicyclists and pedestrians, drainage improvements, and traffic signal modifications.

Board of County Commissioners (BCC) Meeting

Based on the above considerations, County staff brought the recommended build alternative (Typical Section 1A, North Alignment) to the Board of County Commissioner meeting on March 1, 2021. The BCC approved this alternative, thereby allowing the Partin Settlement Road Widening Project to move forward through final design, right-of-way acquisition, and the construction process. Preliminary plans for the preferred Alternative Typical Section 1A, North Alignment are shown in Appendix D of this report.



APPENDIX A: SHPO REVIEW OF CULTURAL RESOURCE ASSESSMENT SURVEY



FLORIDA DEPARTMENT of STATE

RON DESANTIS
Governor

LAUREL M. LEE
Secretary of State

Sara B. McLaughlin
Johnson, Mirmiran & Thompson, Inc.
1600 Market Street, Suite 520
Philadelphia, PA 19103

December 22, 2020

RE: DHR Project File No.: 2020-7348-A

Received by DHR: November 16, 2020

Cultural Resource Assessment Survey for Partin Settlement Road Widening and Reconstruction Project, Osceola County, Florida

Dear Ms. McLaughlin:

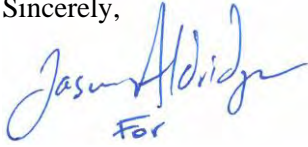
We note that in September 2020, Johnson, Mirmiran & Thompson, Inc. (JMT) conducted the above referenced cultural resource assessment survey (CRAS) as due diligence on behalf of Osceola County. Our office proceeded to review this report with the expectation that Osceola County will be engaging in permitting processes that will require this office to comment on possible adverse impacts to cultural resources listed or eligible for listing in the National Register of Historic Places (NRHP), or otherwise of historical, architectural, or archaeological significance. We recommend at the time such actions are taken, a copy of this letter be forwarded to the permitting agency(ies) with the application. This letter does not constitute a review under Section 106 of the National Historic Preservation Act.

JMT recorded one archaeological site (8OS03098) and two archaeological occurrences (AOs) within the ca. 194.3-acre survey area. Site 8OS03098 is a low density lithic scatter which is recommended as not NRHP eligible. By definition, the two AOs are also not eligible. Three previously recorded archaeological sites (8OS00125, 8OS00126, 8OS01844) are plotted within the APE but were not observed during the current survey and appear to have been destroyed by development. These three sites were previously determined as not eligible for listing in the NRHP. Two previously recorded historic structures (the Charles Partin House, 8OS1963; and the Charles Partin Outbuilding, 8OS2077) and one resource group, St. Cloud and Sugar Belt Railway (8OS2822) are also in the project vicinity and have likewise been determined not NRHP eligible. No other cultural resources were encountered. JMT proposes that this project will have no effect on cultural resources listed, or eligible for listing in the NRHP, or otherwise of archaeological, historical, or architectural significance within the survey area. JMT recommends no additional work in the APE.

We find the submitted report complete and sufficient in accordance with Chapter 1A-46, *Florida Administrative Code*. Because this project is not yet subject to permitting requirements from a state or federal agency, our office abstains from supplying a determination of effects until such time that permitting agencies (and associated permit requirements) are identified for this project.

If I can be of any further help, or if you have any questions about this letter, please feel free to contact Clete Rooney at Cletus.Rooney@dos.myflorida.com.

Sincerely,

A handwritten signature in blue ink that reads "Jason Aldridge". Below the signature, the word "For" is written in a smaller, cursive script.

Timothy A. Parsons, Ph.D.
Director, Division of Historical Resources
and State Historic Preservation Officer



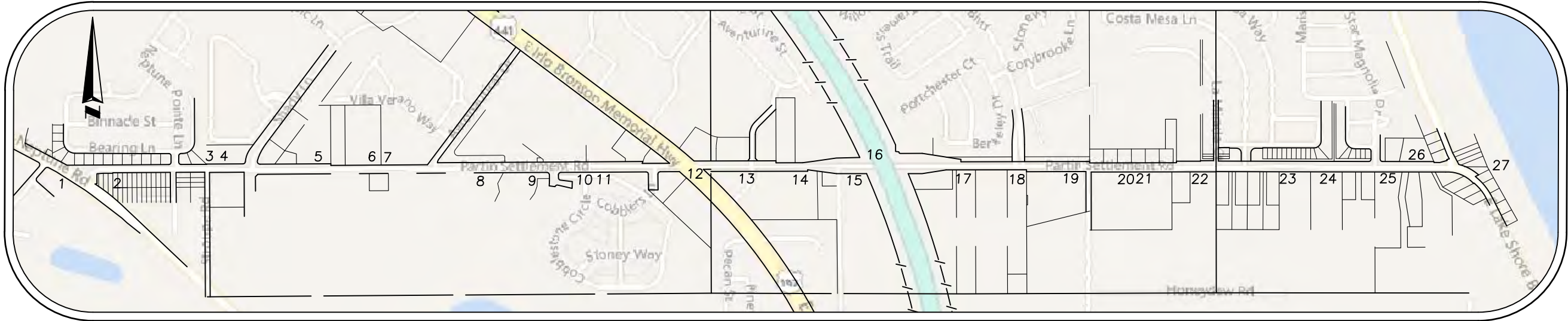
APPENDIX B: EXISTING RIGHT-OF-WAY AND UTILITY MAPS

PARTIN SETTLEMENT ROAD

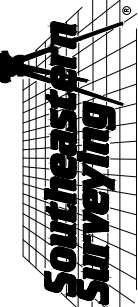
PRELIMINARY RIGHT OF WAY EXHIBIT

SHEET LAYOUT

SHEET 1: COVER SHEET
SHEETS 2-28: RIGHT OF WAY DETAILS



EDWIN MUNOZ JR., PSM
Registered Land Surveyor
Number 7288



**SOUTHEASTERN SURVEYING
AND MAPPING CORPORATION**
6500 All American Boulevard Orlando,
Florida 32810-4350 (407) 292-8580
Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Certified to:

Field Date: April 21, 2021

Drawn By: WMS

Scale:

1" = 40'

**DRAWING NUMBER
64576 PRELIM RW**

**SHEET
NUMBER**

1 OF 28

LOT 12

NEPTUNE POINTE
PLAT BOOK 16, PAGES 15-16

LOT 11

LOT 10

LOT 9

LOT 8

LOT 7

APPARENT RIGHT OF WAY LINE
ELECTRIC

FIBER OPTIC
ELECTRIC

WATER

WATER

APPARENT RIGHT OF WAY LINE

BE BTE
FOC BTE

WATER

116.8' (CALC)

PARTIN SETTLEMENT ROAD

RIGHT OF WAY VARIES
PER PLAT BOOK 16, PAGES 15-16

87.0' (FIELD)

10+00 10+50 11+00 11+50 12+00 12+50 13+00

SURVEY BASELINE N89°36'27"E

APPARENT RIGHT OF WAY LINE
OHL

OHL

TRAFFIC

FOUND 5/8" IRON ROD
UNREADABLE

FOUND 5/8" IRON ROD
STAMPED "LB 966"

2'X2' CONCRETE

APPARENT RIGHT OF WAY LINE

LOT 2

LOT 3

LOT 4

BLOCK K
MIDWAY CITY TOWNSITE
PLAT BOOK 1, PAGE 102

PARCEL ID:
25-25-29-4140-000K-0010

NEPTUNE ROAD
RIGHT OF WAY VARIES
PER PLAT BOOK 16, PAGES 15-16

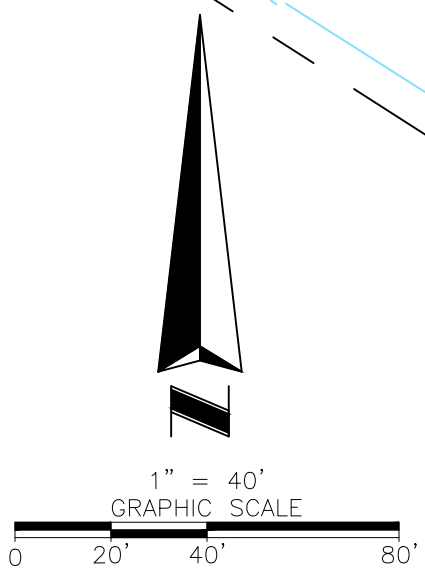
TRAFFIC

FOUND 1/2" IRON ROD
STAMPED "LB 966"

FOUND 1/2" IRON ROD
STAMPED "LB 966"

APPARENT RIGHT OF WAY LINE

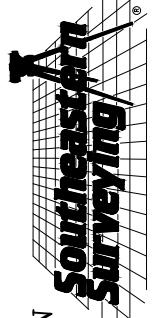
114.6' (CALC)



SEE SHEET 1

SEE SHEET 2

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



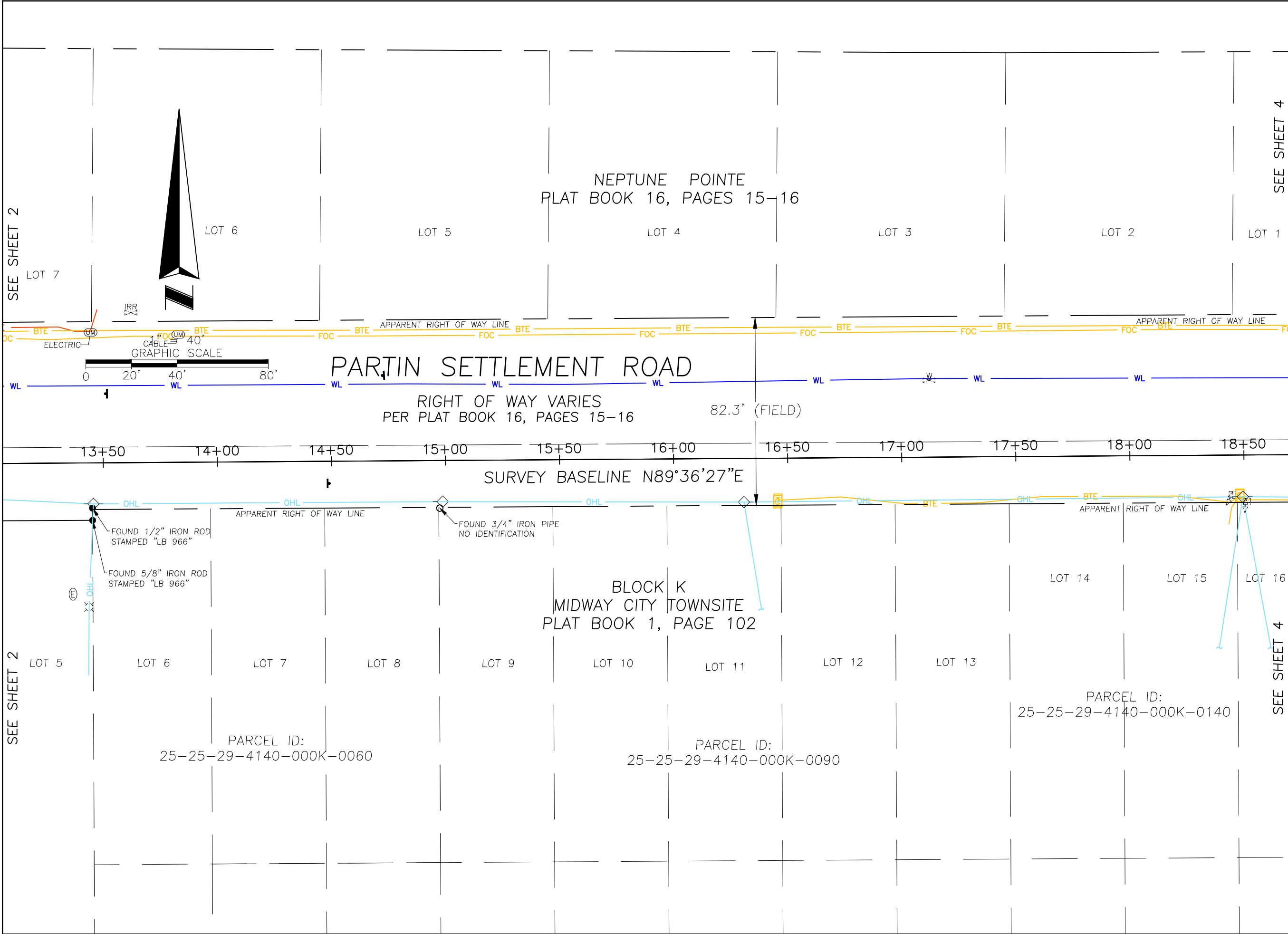
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AND MAPPING CORPORATION
6500 All American Boulevard Orlando,
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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

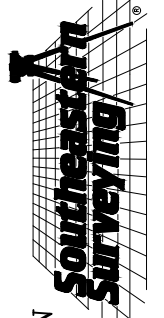
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Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
2 OF 28



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

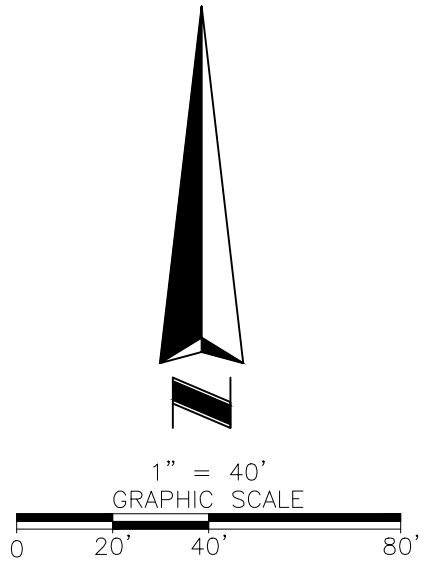
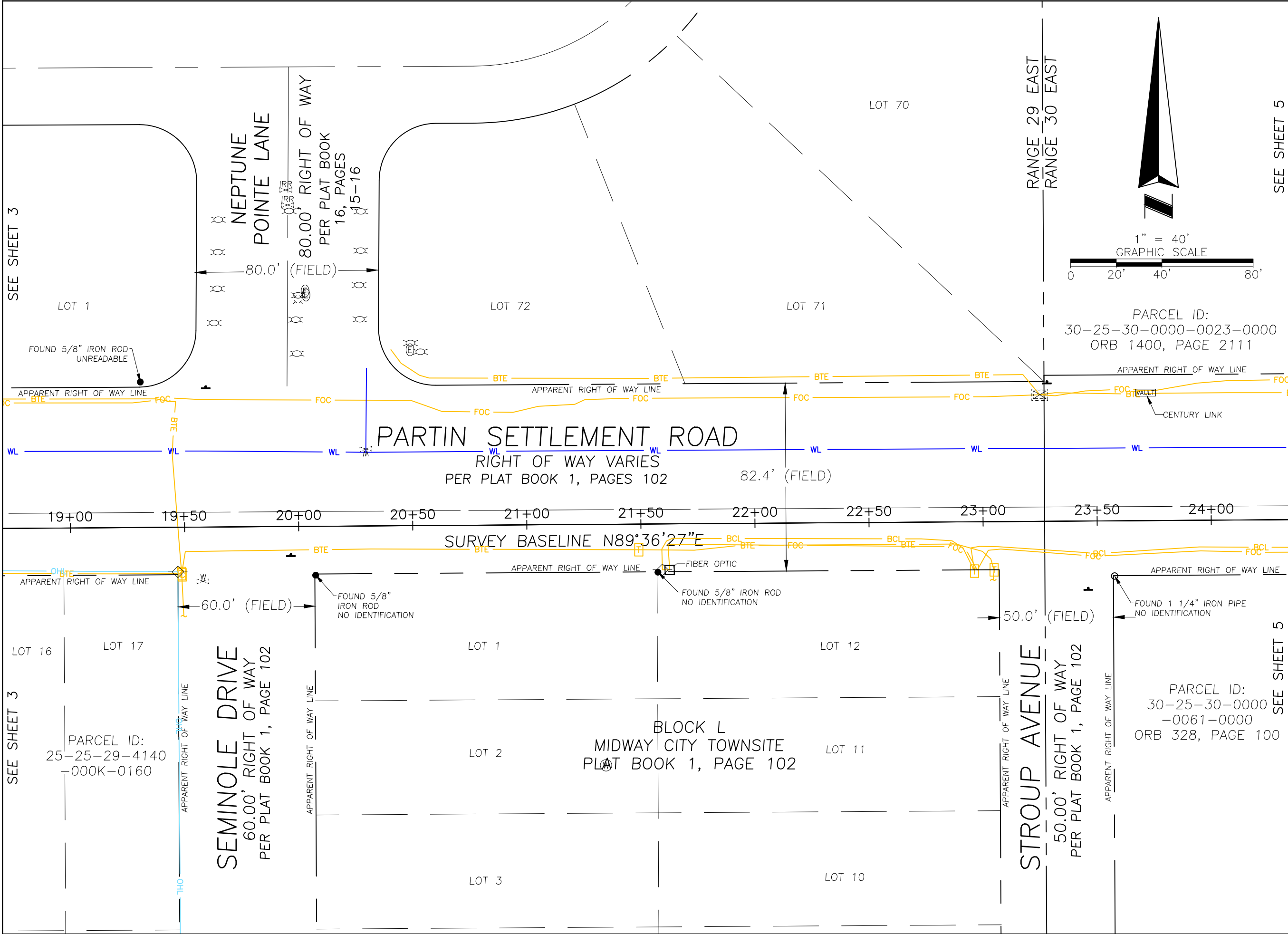
Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET NUMBER
3 OF 28



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ORB 1400, PAGE 2111

PARCEL ID:
30-25-30-0000-0061-0000
ORB 328, PAGE 100

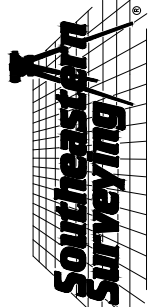
SEE SHEET 3

SEE SHEET 3

SEE SHEET 5

SEE SHEET 5

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

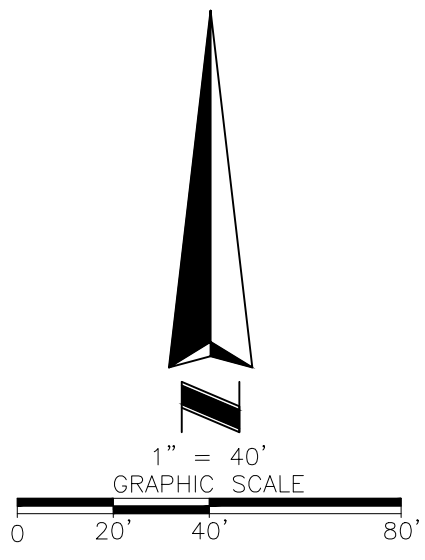
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Drawn By: WMS
Scale: 1" = 40'

**DRAWING NUMBER
64576 PRELIM RW**

**SHEET
NUMBER
4 OF 28**

SEE SHEET 4

SEE SHEET 4



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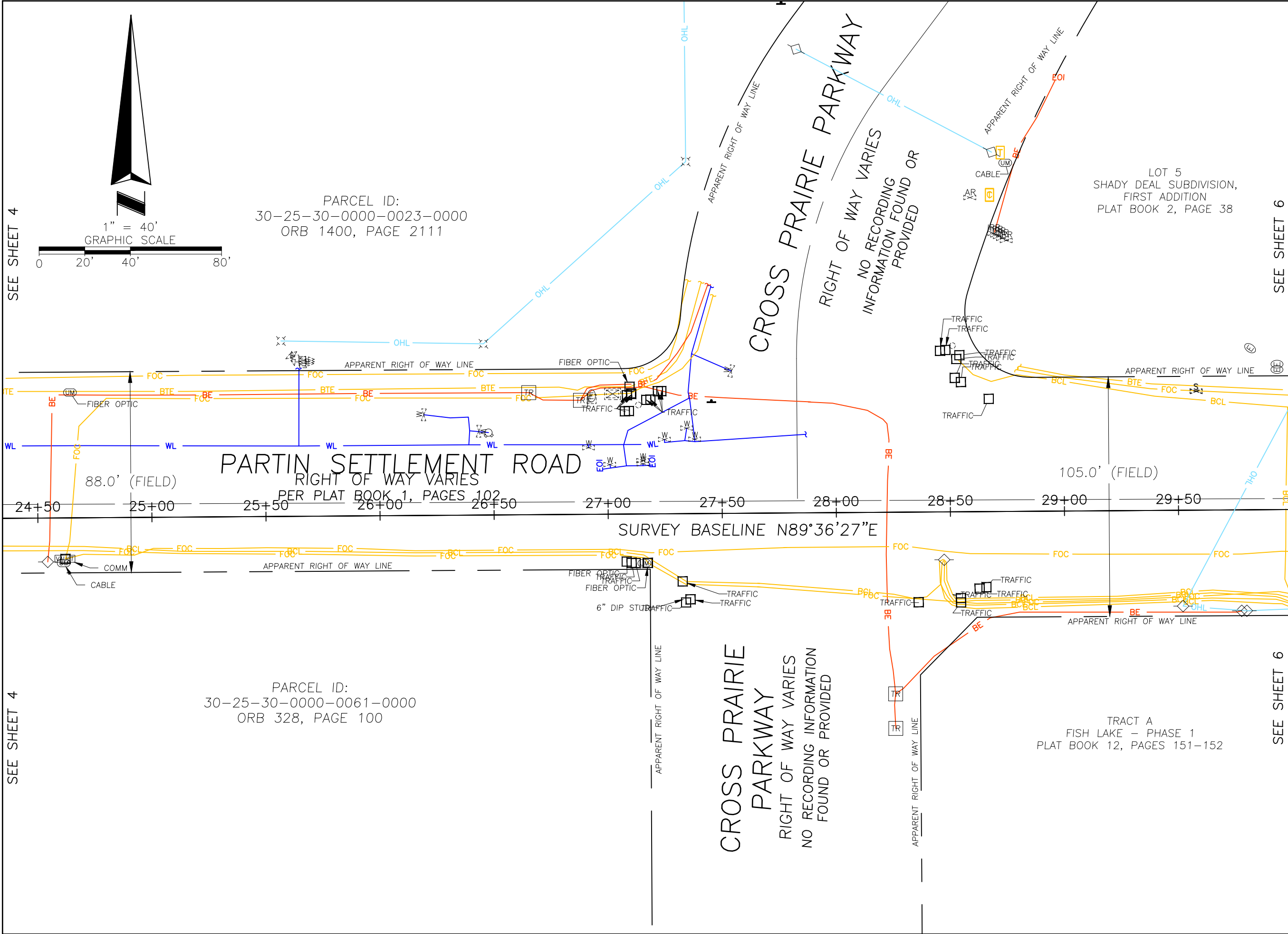
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30-25-30-0000-0061-0000
ORB 328, PAGE 100

CROSS PRAIRIE PARKWAY
RIGHT OF WAY VARIES
NO RECORDING INFORMATION FOUND OR PROVIDED

CROSS PRAIRIE PARKWAY
RIGHT OF WAY VARIES
NO RECORDING INFORMATION FOUND OR PROVIDED

LOT 5
SHADY DEAL SUBDIVISION,
FIRST ADDITION
PLAT BOOK 2, PAGE 38

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152



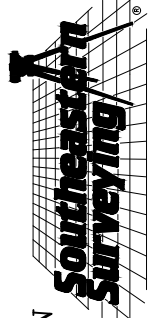
SEE SHEET 6

SEE SHEET 6

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Certification Number LB2108 e-mail:
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SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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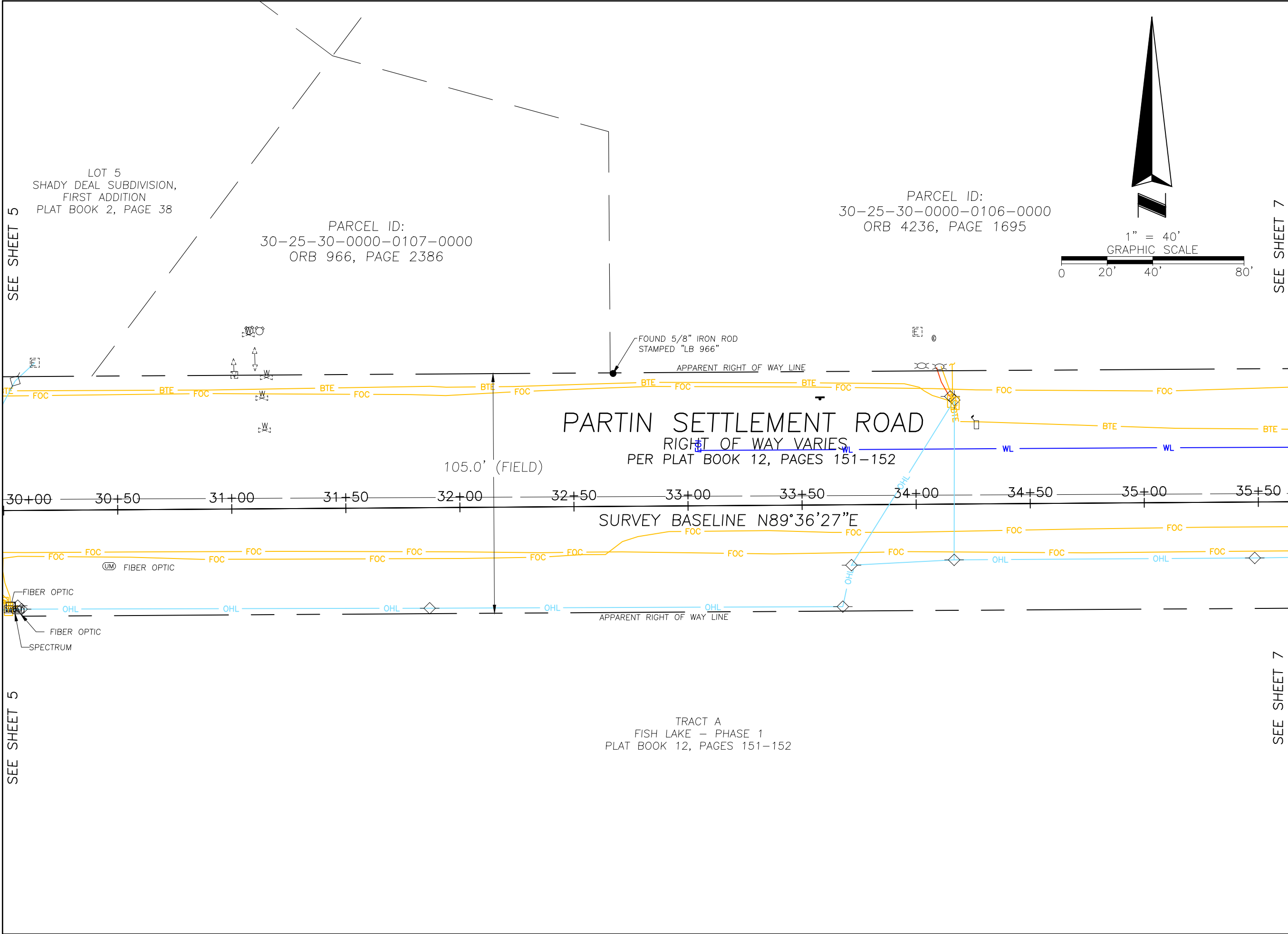
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Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
5 OF 28



SEE SHEET 5

SEE SHEET 5

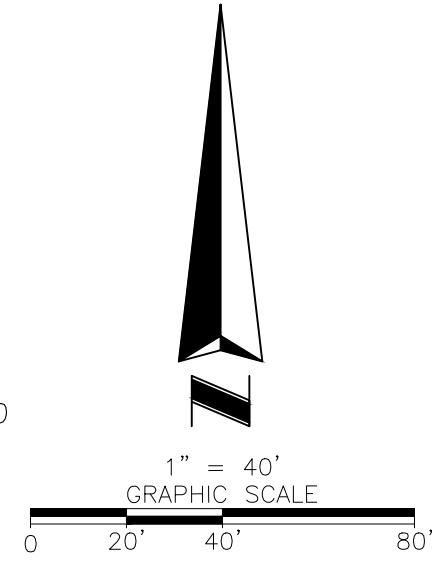
SEE SHEET 7

SEE SHEET 7

LOT 5
SHADY DEAL SUBDIVISION,
FIRST ADDITION
PLAT BOOK 2, PAGE 38

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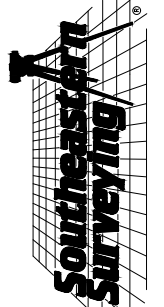


PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES
PER PLAT BOOK 12, PAGES 151-152

SURVEY BASELINE N89°36'27"E

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
6 OF 28

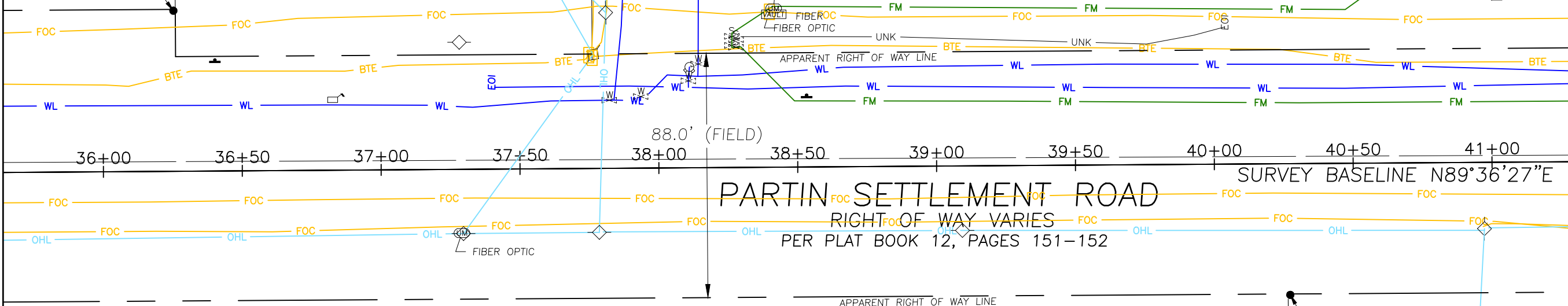
SEE SHEET 6

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30-25-30-0000-0105-0000
ORB 2537, PAGE 1850

PARCEL ID:
30-25-30-0000-0035-0000
ORB 1382, PAGE 1470

SEE SHEET 8

FOUND 5/8" IRON ROD
NO IDENTIFICATION



PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES
PER PLAT BOOK 12, PAGES 151-152

SURVEY BASELINE N89°36'27"E

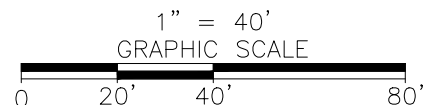
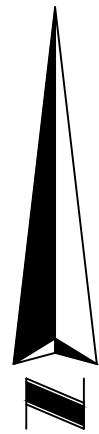
88.0' (FIELD)

FOUND 5/8" IRON ROD
LB 966

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ORB 1144, PAGE 2910

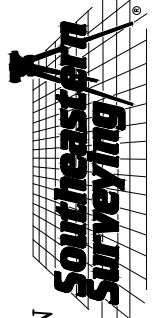
SEE SHEET 6

SEE SHEET 8



TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET
NUMBER

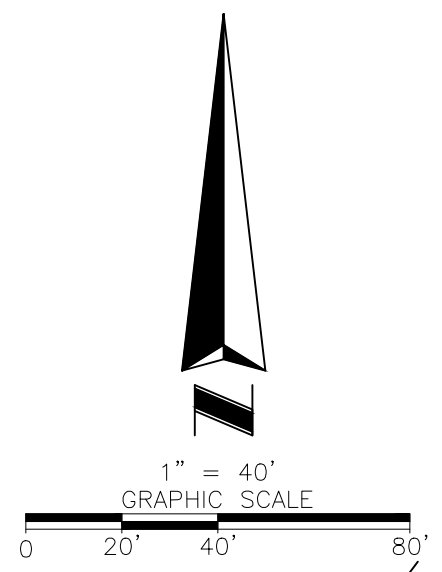
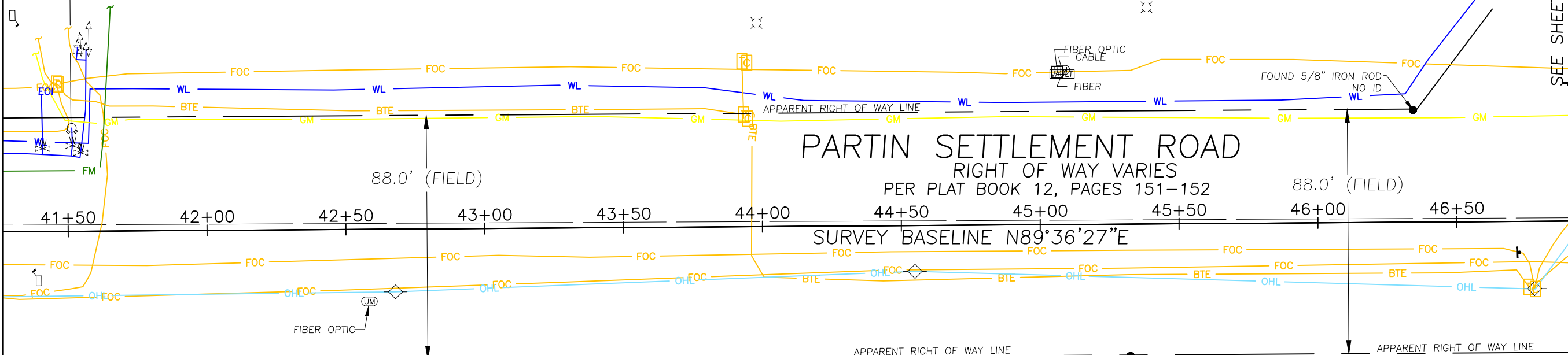
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SEE SHEET 7

SEE SHEET 7

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ORB 1542, PAGE 1366



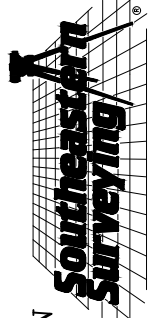
SEE SHEET 9

PARCEL ID:
30-25-30-0000-0070-0000
ORB 1144, PAGE 2910

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

SEE SHEET 9

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



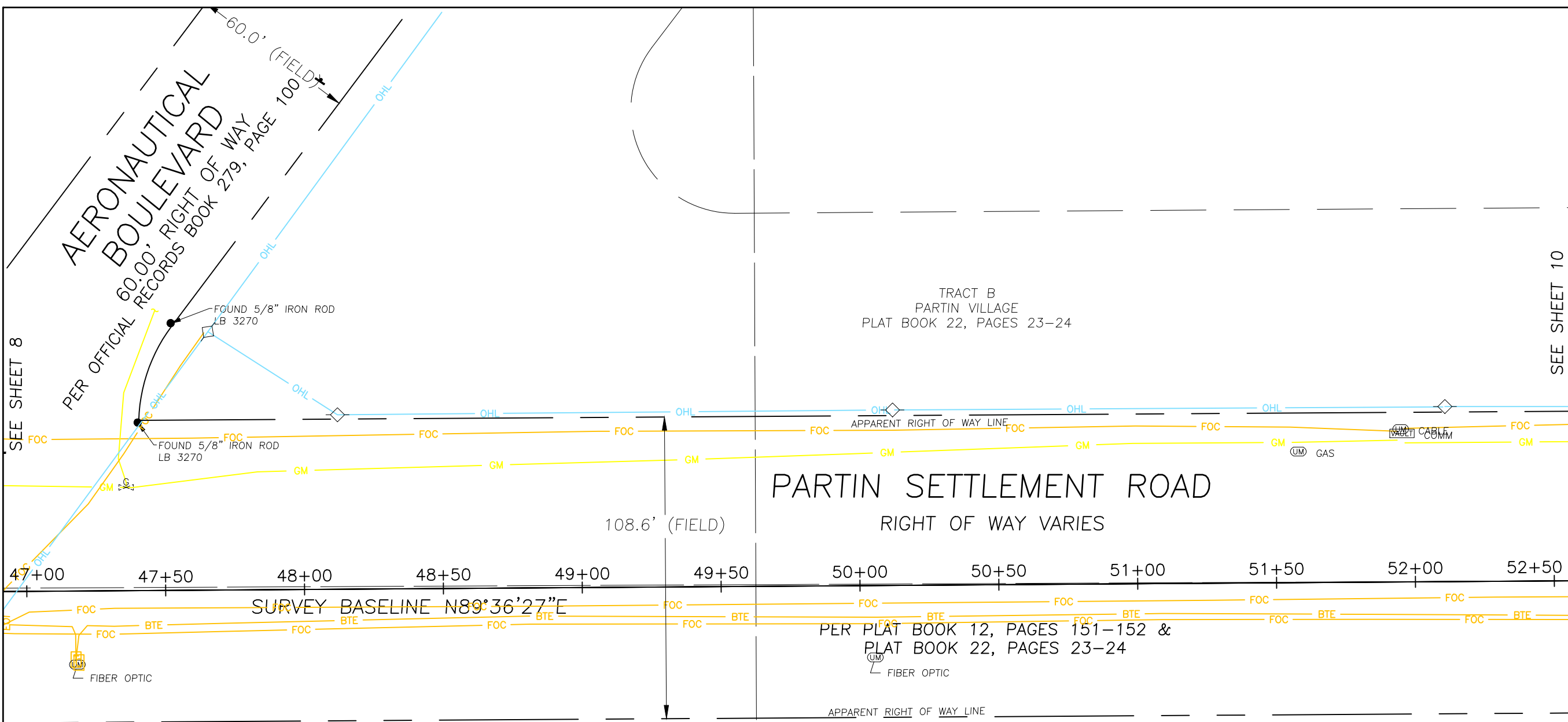
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Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Partin Settlement Road

Project: Partin Settlement Road
Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
8 OF 28



SEE SHEET 8

SEE SHEET 8

SEE SHEET 10

SEE SHEET 10

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

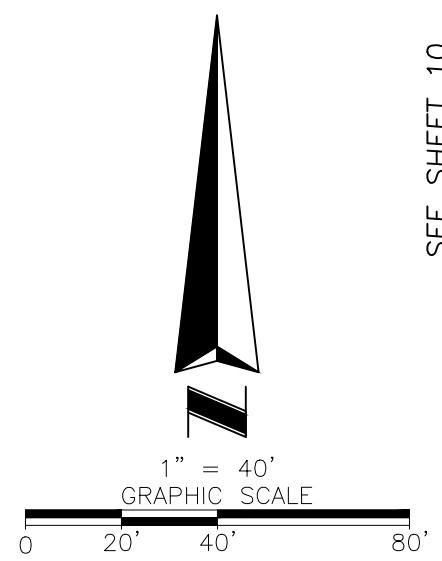
TRACT B
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

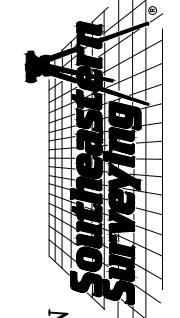
PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES

SURVEY BASELINE N89°36'27"E

PER PLAT BOOK 12, PAGES 151-152 &
PLAT BOOK 22, PAGES 23-24



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT
Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
9 OF 28

SEE SHEET 9

SEE SHEET 9

SEE SHEET 11

SEE SHEET 11

TRACT B
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

TRACT SW
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

LOT 6
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

TRACT C
PARTIN VILLAGE
PLAT BOOK 22
PAGES 23-24

PARTIN SETTLEMENT ROAD

RIGHT OF WAY VARIES
PER PLAT BOOK 12, PAGES 151-152
& PLAT BOOK 22, PAGES 23-24

SURVEY BASELINE N89°36'27"E

53+00 53+50 54+00 54+50 55+00 55+50 56+00 56+50 57+00 57+50 58+00

108.6' (FIELD)

FOUND NAIL & DISK
PCP LB 966

APPARENT RIGHT OF WAY LINE

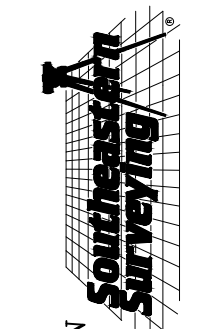
FIBER OPTIC

FIBER OPTIC COMM

TRACT A
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

TRACT B
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



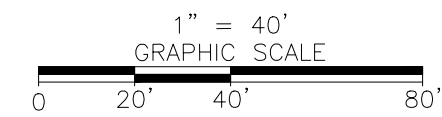
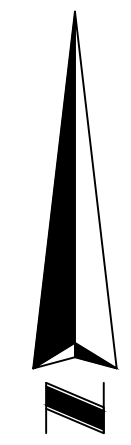
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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
10 OF 28

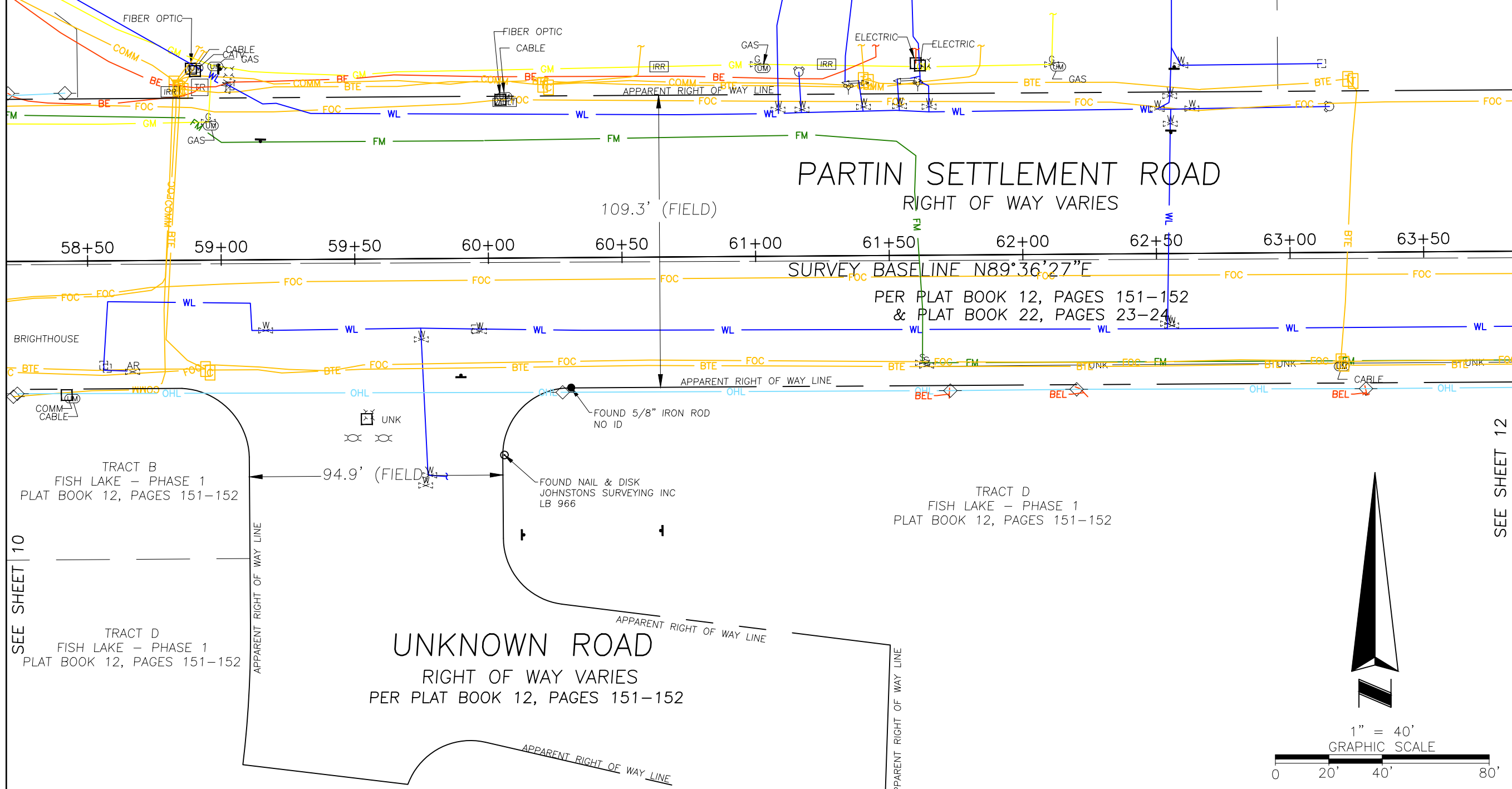


SEE SHEET 10

SEE SHEET 10

LOT 6
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

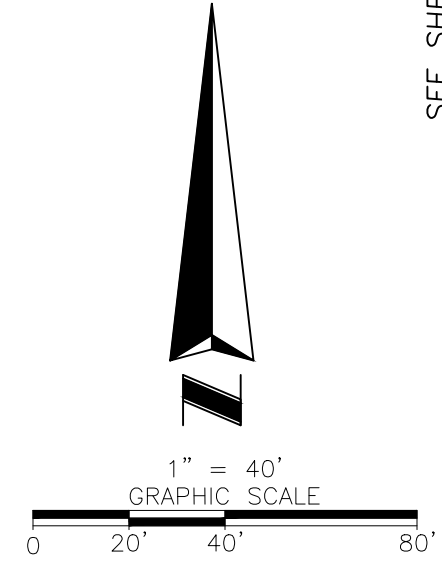
LOT 1
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24



PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES

UNKNOWN ROAD
RIGHT OF WAY VARIES
PER PLAT BOOK 12, PAGES 151-152

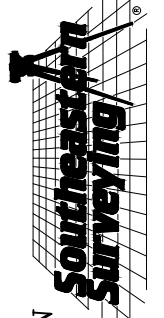
SURVEY BASELINE N89°36'27"E
PER PLAT BOOK 12, PAGES 151-152
& PLAT BOOK 22, PAGES 23-24



SEE SHEET 12

SEE SHEET 12

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Partin Settlement Road

Project: Partin Settlement Road

Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET
NUMBER
11 OF 28

SEE SHEET 11

SEE SHEET 11

SEE SHEET 13

SEE SHEET 13

LOT 1
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

LOT 1
PARTIN CORNERS
PLAT BOOK 20, PAGE 115

PARTIN SETTLEMENT ROAD

RIGHT OF WAY VARIES

PER PLAT BOOK 12, PAGES 151-152
& PLAT BOOK 22, PAGES 23-24

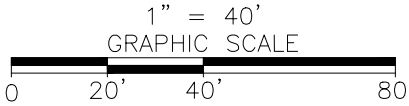
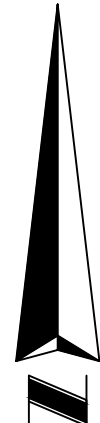
TRACT C
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

64+00 64+50 65+00 65+50 66+00 66+50 67+00 67+50 68+00 68+50 69+00

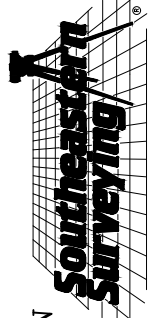
109.3' (FIELD)

88.5' (FIELD)

SURVEY BASELINE N89°36'27" E



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



SOUTHEASTERN SURVEYING
AND MAPPING CORPORATION

6500 All American Boulevard Orlando,
Florida 32810-4350 (407) 292-8580
Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

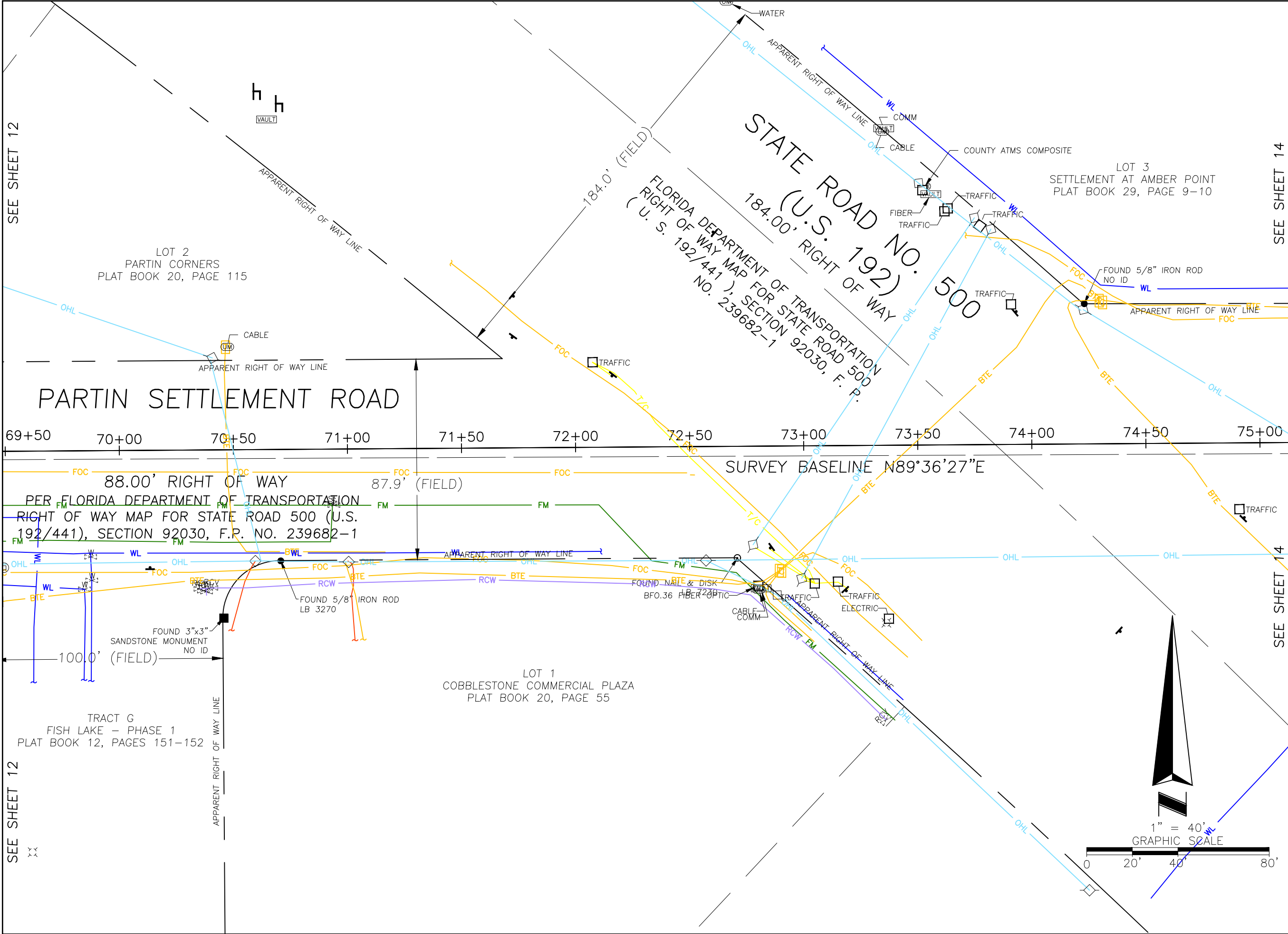
DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
12 OF 28

SEE SHEET 12

SEE SHEET 12

SEE SHEET 14

SEE SHEET 14



LOT 2
PARTIN CORNERS
PLAT BOOK 20, PAGE 115

PARTIN SETTLEMENT ROAD

STATE ROAD NO. 500
(U.S. 192)
184.00' RIGHT OF WAY
FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 500
(U.S. 192/441), SECTION 92030, F.P. NO. 239682-1

LOT 3
SETTLEMENT AT AMBER POINT
PLAT BOOK 29, PAGE 9-10

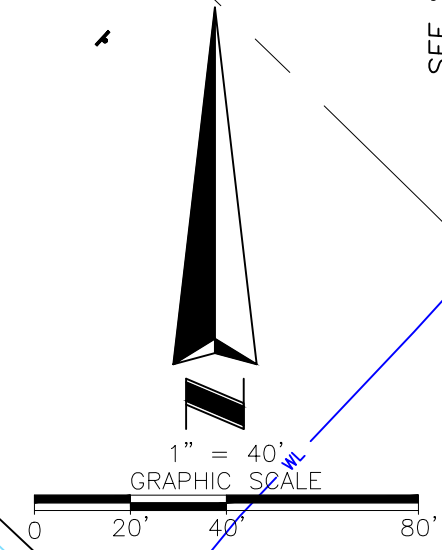
88.00' RIGHT OF WAY
87.9' (FIELD)

PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 500 (U.S. 192/441), SECTION 92030, F.P. NO. 239682-1

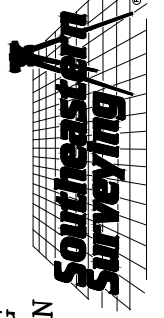
SURVEY BASELINE N89°36'27"E

LOT 1
COBBLESTONE COMMERCIAL PLAZA
PLAT BOOK 20, PAGE 55

TRACT G
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

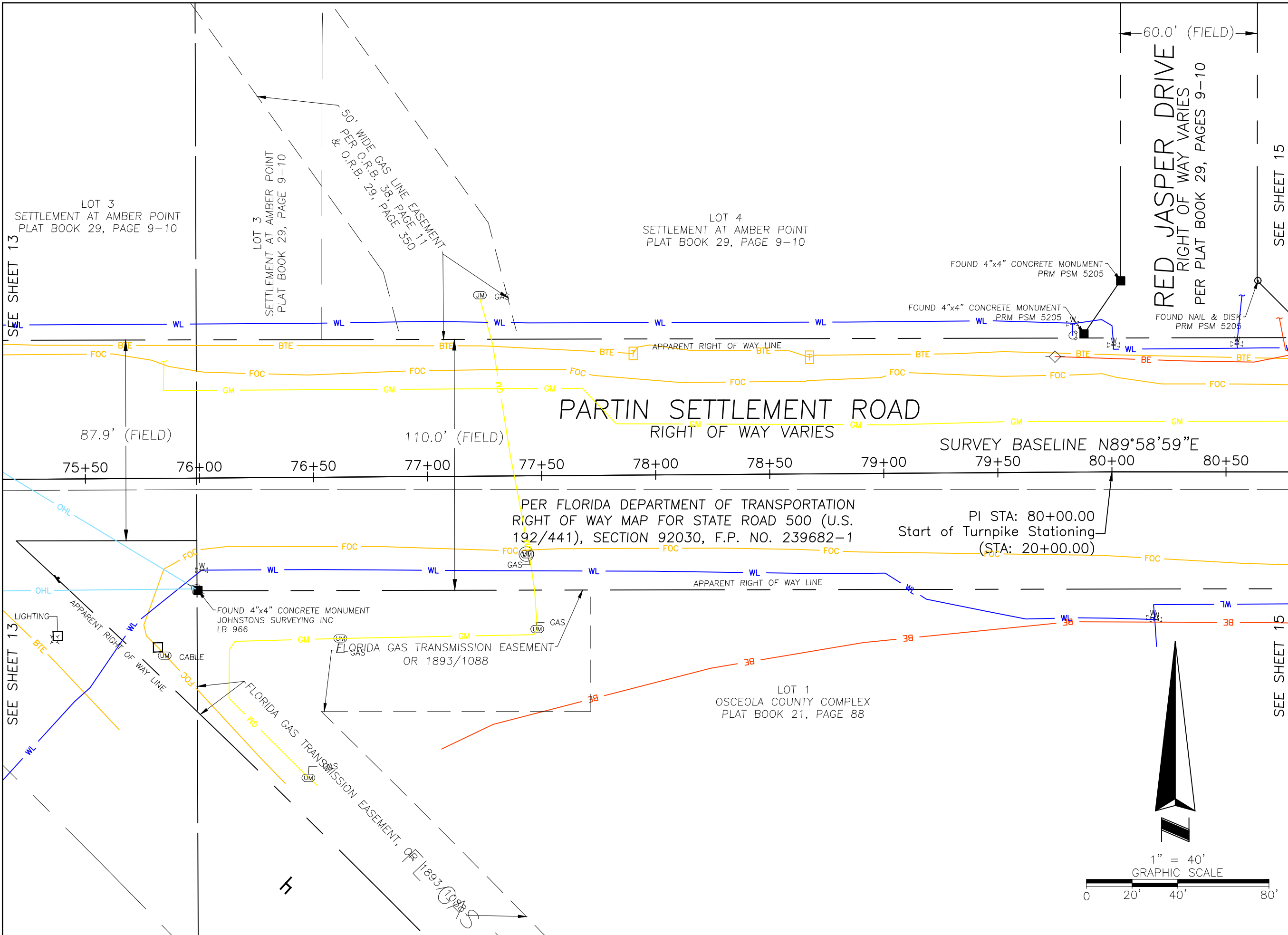
Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET
NUMBER
13 OF 28



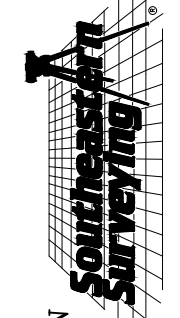
SEE SHEET 13

SEE SHEET 13

SEE SHEET 15

SEE SHEET 15

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.

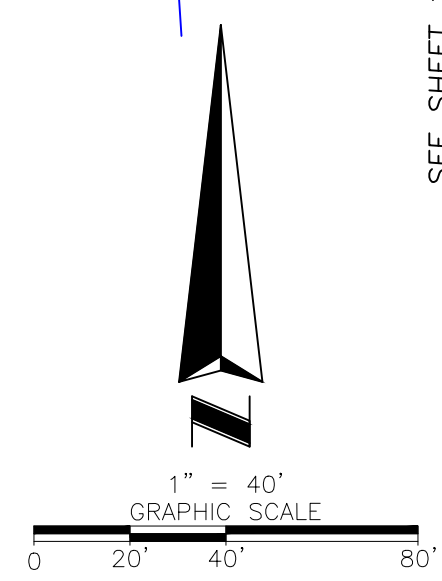


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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT
Partin Settlement Road

Project:	Partin Settlement Road
Field Date:	April 21, 2021
Drawn By:	WMS
Scale:	1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
14 OF 28



SEE SHEET 14

SEE SHEET 14

SEE SHEET 16

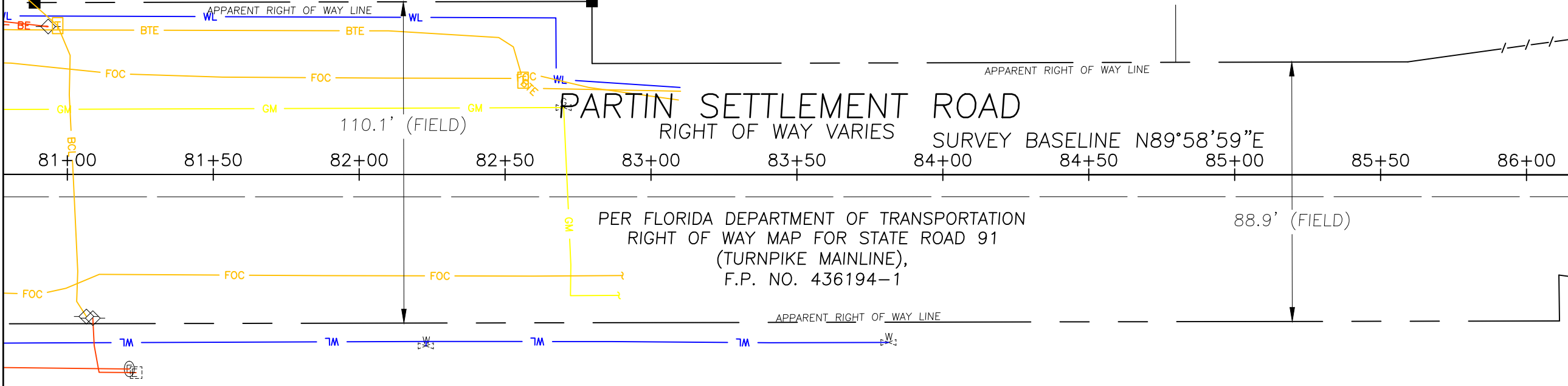
SEE SHEET 16

LOT 5
SETTLEMENT AT AMBER POINT
PLAT BOOK 29, PAGE 9-10

LOT 1
OSCEOLA COUNTY COMPLEX
PLAT BOOK 21, PAGE 88

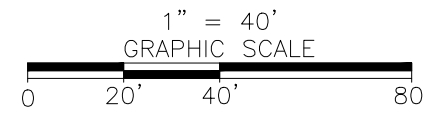
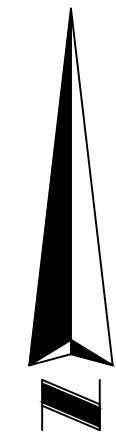
FOUND 4"x4" CONCRETE MONUMENT
PRM PSM 5205

FOUND 4"x4" CONCRETE MONUMENT
PRM PSM 5205

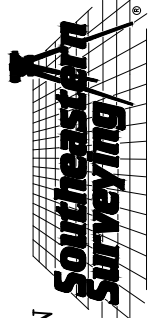


PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES SURVEY BASELINE N89°58'59"E

PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Certification Number LB2108 e-mail:
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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET NUMBER
15 OF 28

SEE SHEET 15

SEE SHEET 15

LOT 5
SETTLEMENT AT AMBER POINT
PLAT BOOK 29, PAGE 9-10

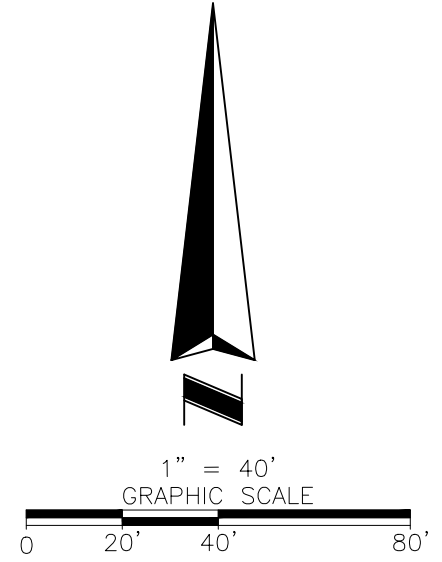
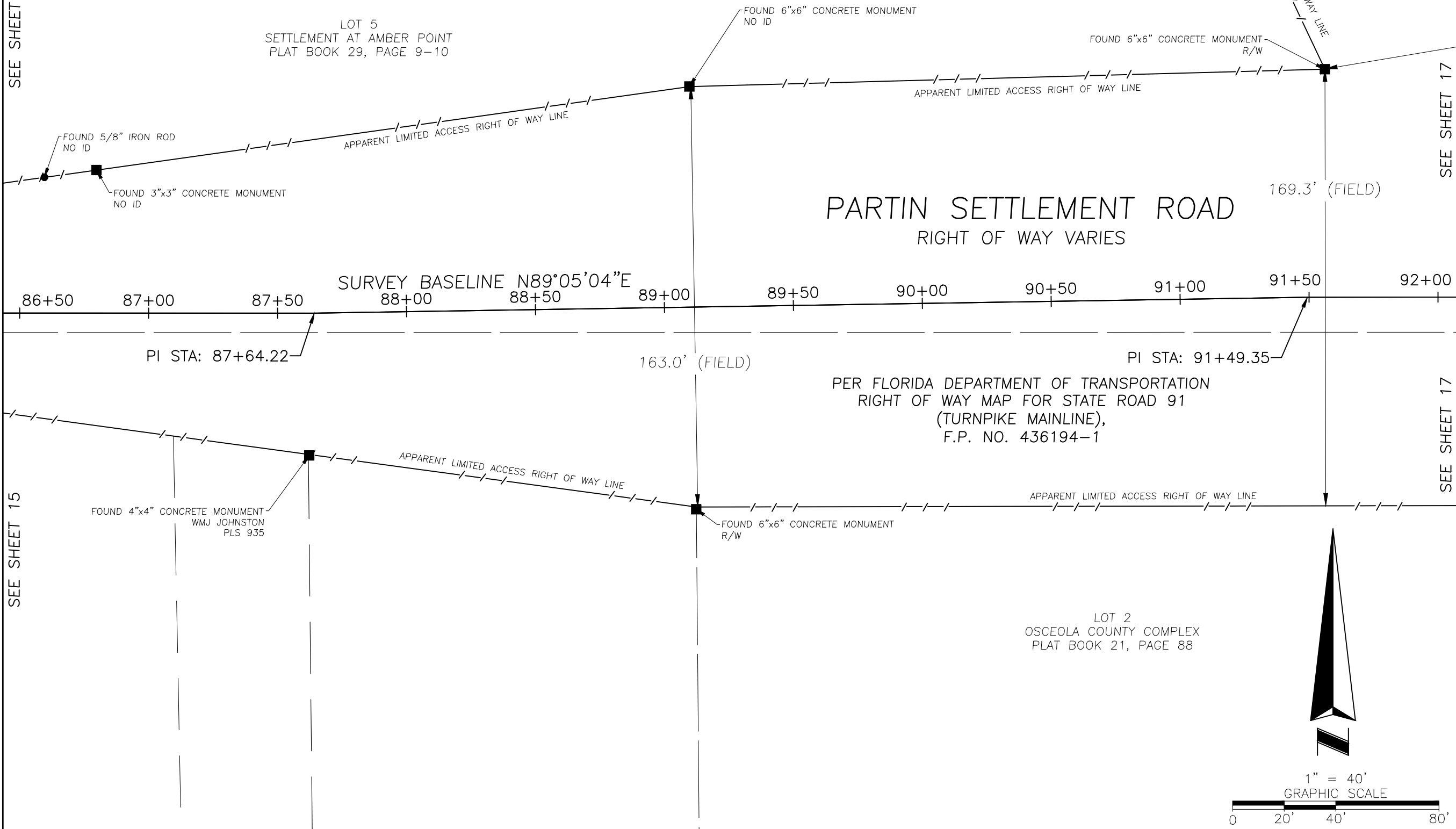
LOT 2
OSCEOLA COUNTY COMPLEX
PLAT BOOK 21, PAGE 88

PARTIN SETTLEMENT ROAD

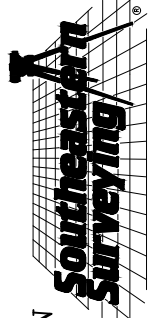
RIGHT OF WAY VARIES

SURVEY BASELINE N89°05'04"E

PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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 info@southeasternsurveying.com

SEE SHEET 17

SEE SHEET 17

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

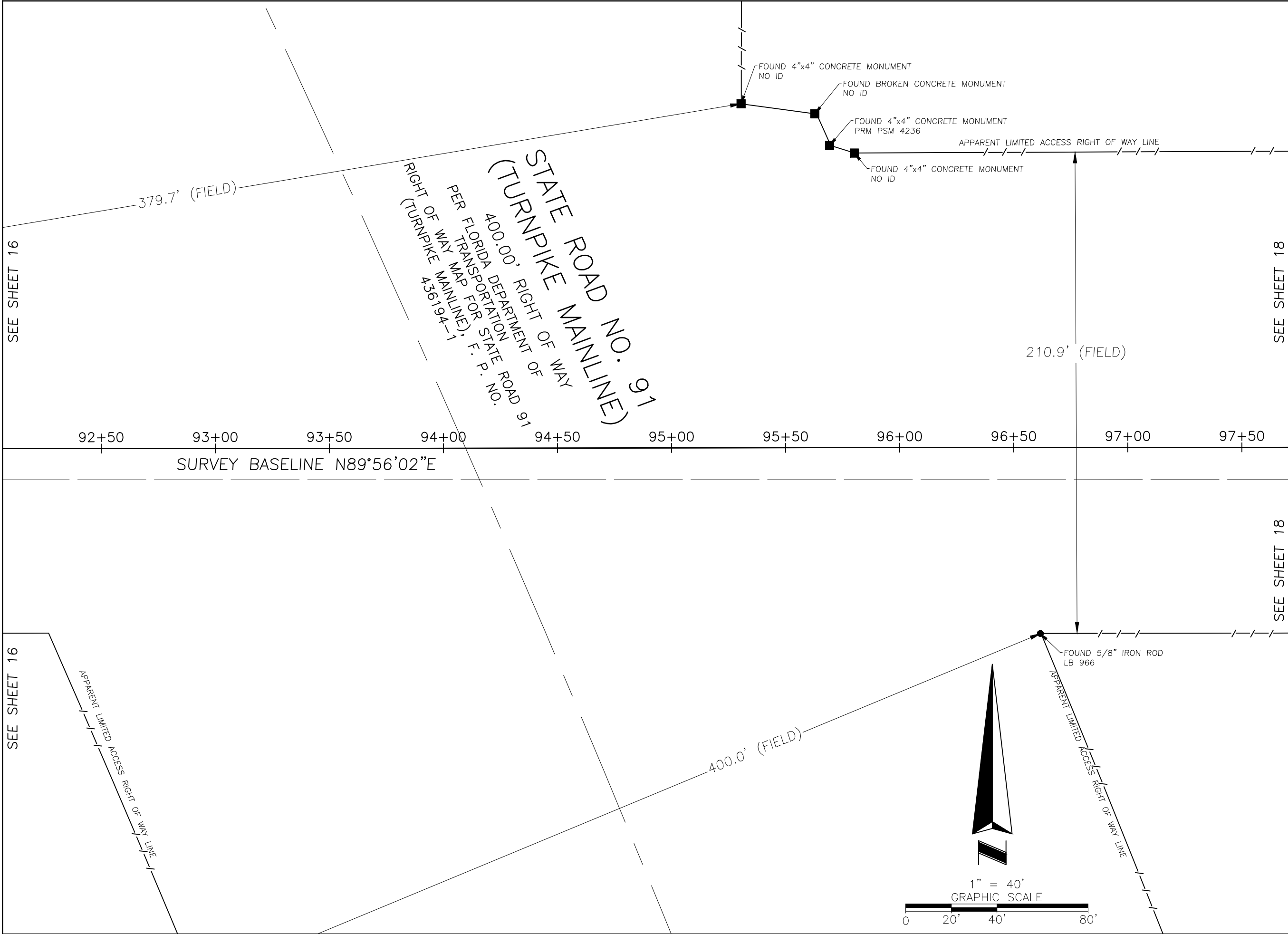
Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET
NUMBER
16 OF 28



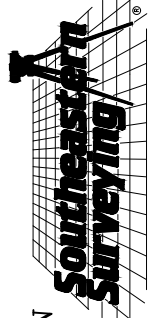
SEE SHEET 16

SEE SHEET 16

SEE SHEET 18

SEE SHEET 18

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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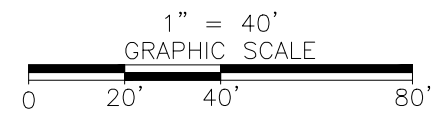
PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

**DRAWING NUMBER
64576 PRELIM RW**

**SHEET
NUMBER
17 OF 28**



SEE SHEET 17

SEE SHEET 17

SEE SHEET 19

SEE SHEET 19

FOUND 4"x4" CONCRETE MONUMENT
PRM PSM 4236

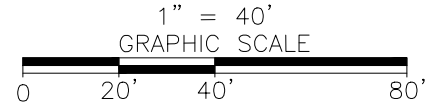
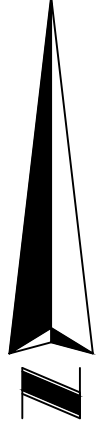
FOUND 4"x4" CONCRETE MONUMENT
PRM PSM 4236

FOUND 1/2" IRON ROD
LB 5824 GBA

FOUND 5/8" IRON ROD
LB 966

FOUND NAIL & DISK
JOHNSTONS SURVEYING INC
LB 966

FOUND 5/8" IRON ROD
LB 966



APPARENT LIMITED ACCESS RIGHT OF WAY LINE

APPARENT RIGHT OF WAY LINE

APPARENT RIGHT OF WAY LINE

SURVEY BASELINE N89°16'33"E

PI STA: 98+23.16

PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES
PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1

PARCEL ID:
29-25-30-0000-0174-0000
ORB 5223, PAGE 17

PARCEL ID:
29-25-30-0000-0170-0000
ORB 5223, PAGE 17

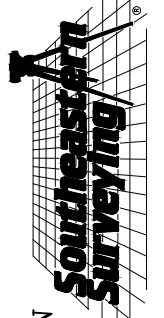
PARCEL ID:
29-25-30-0000
-0165-0000
ORB 5223, PAGE 17

211.3' (FIELD)

85.1' (FIELD)

98+00 98+50 99+00 99+50 100+00 100+50 101+00 101+50 102+00 102+50 103+00

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
18 OF 28

SEE SHEET 18

SEE SHEET 18

SOMERSET
PLAT BOOK 16, PAGES 178-179

TRACT "D"
REMINGTON - PHASE 2

REMINGTON
BOULEVARD

RIGHT OF WAY VARIES
PER PLAT BOOK 11, PAGES 28-29

PARTIN SETTLEMENT ROAD

PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1

PI STA: 108+82.68
End of Turnpike Stationing
(STA: 48+82.68)

SURVEY BASELINE N89°16'33"E
103+50 104+00 104+50

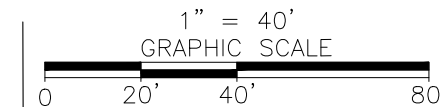
PI STA: 104+81.40

SURVEY BASELINE N89°44'49"E

85.2' (FIELD)

FOUND 1/2" IRON ROD
NO IDENTIFICATION

FOUND 5/8" IRON ROD
NO IDENTIFICATION



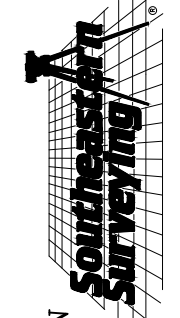
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29-25-30-0000-0160-0000
ORB 5301, PAGE 237

PARCEL ID:
29-25-30-0000-0161-0000
ORB 2223, PAGES 2070-2073

SEE SHEET 20

SEE SHEET 20

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW

SHEET NUMBER

19 OF 28

PARCEL "L"
 REMINGTON - PHASE 2
 PLAT BOOK 11, PAGES 28-29

TRACT "E"
 REMINGTON - PHASE 2

PARTIN SETTLEMENT ROAD
 RIGHT OF WAY VARIES

SURVEY BASELINE N89°49'20"E
 PER FLORIDA DEPARTMENT OF TRANSPORTATION
 RIGHT OF WAY MAP FOR STATE ROAD 91
 (TURNPIKE MAINLINE),
 F.P. NO. 436194-1

PARCEL ID:
 29-25-30-0000-0145-0000
 ORB 5134, PAGE 603

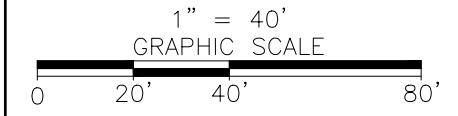
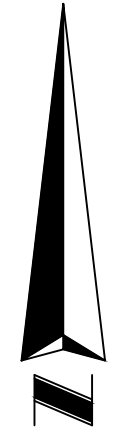
SEE SHEET 19

SEE SHEET 21

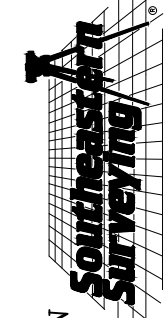
10" DIP WATER SERVICES

SEE SHEET 19

FOUND 5/8" IRON ROD
 NO IDENTIFICATION



SEE SHEET 1 FOR NOTES,
 LEGEND AND DESCRIPTION.



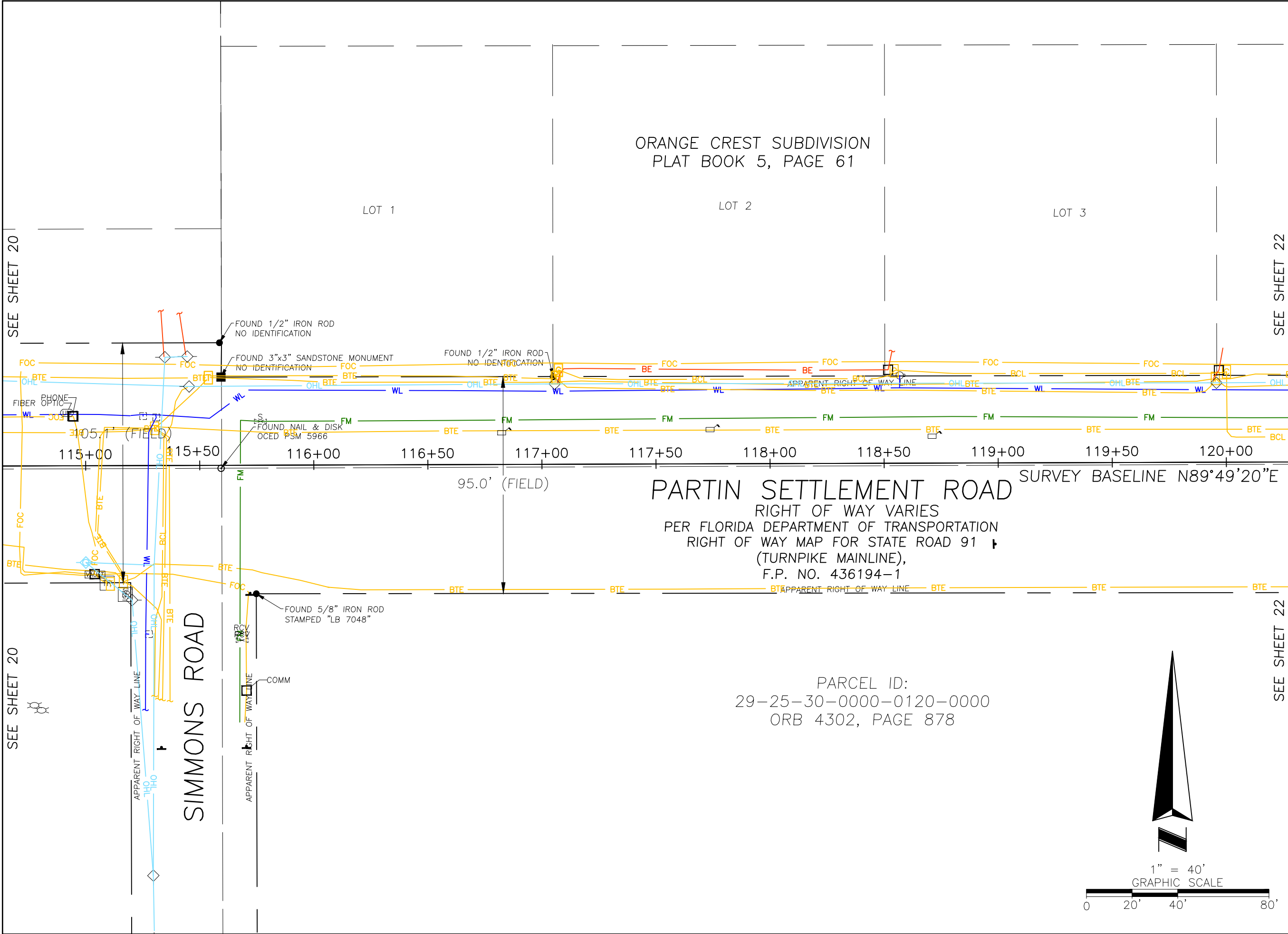
**SOUTHEASTERN SURVEYING
 AND MAPPING CORPORATION**
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 Florida 32810-4350 (407) 292-8580
 Certification Number LB2108 e-mail:
 info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
 Drawn By: WMS
 Scale: 1" = 40'

DRAWING NUMBER
 64576 PRELIM RW
SHEET NUMBER
 20 OF 28



ORANGE CREST SUBDIVISION
PLAT BOOK 5, PAGE 61

LOT 1

LOT 2

LOT 3

SEE SHEET 20

SEE SHEET 22

SEE SHEET 20

SEE SHEET 22

PARTIN SETTLEMENT ROAD

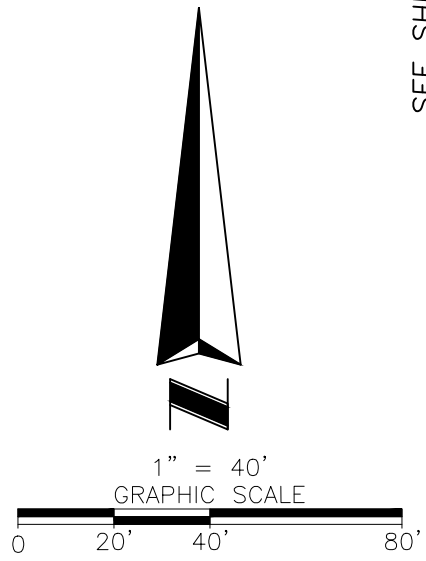
RIGHT OF WAY VARIES
PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1

SURVEY BASELINE N89°49'20"E

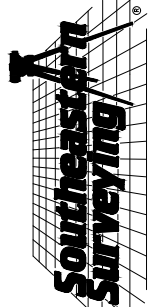
95.0' (FIELD)

PARCEL ID:
29-25-30-0000-0120-0000
ORB 4302, PAGE 878

SIMMONS ROAD



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

**DRAWING NUMBER
64576 PRELIM RW**

**SHEET
NUMBER
21 OF 28**

SEE SHEET 21

SEE SHEET 21

SEE SHEET 23

SEE SHEET 23

ORANGE CREST SUBDIVISION
PLAT BOOK 5, PAGE 61

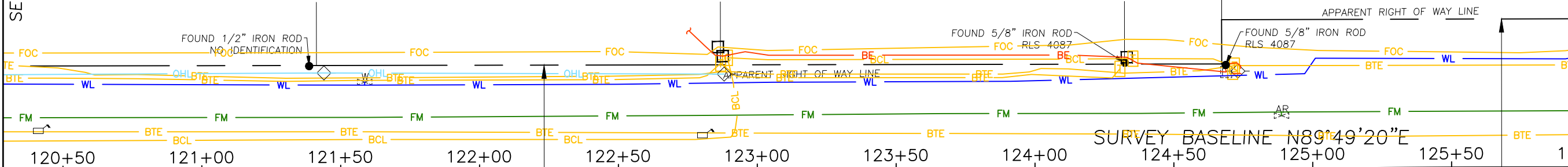
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29-25-30-0000-0075-0000
ORB 1040, PAGE 618

LOT 4

LOT 5

LOT 6

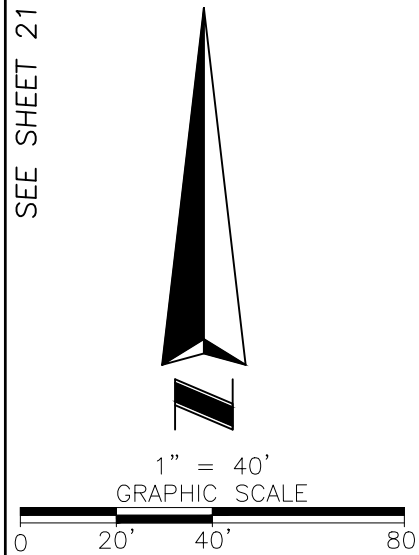
LOT 7



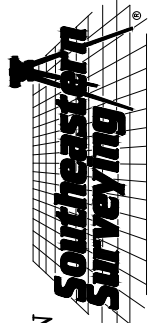
PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES
PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1

PARCEL ID:
29-25-30-0000-0120-0000
ORB 4302, PAGE 878

PARCEL ID:
29-25-30-0000-0115-0000
ORB 5438, PAGE 1903



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Florida 32810-4350 (407) 292-8580
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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

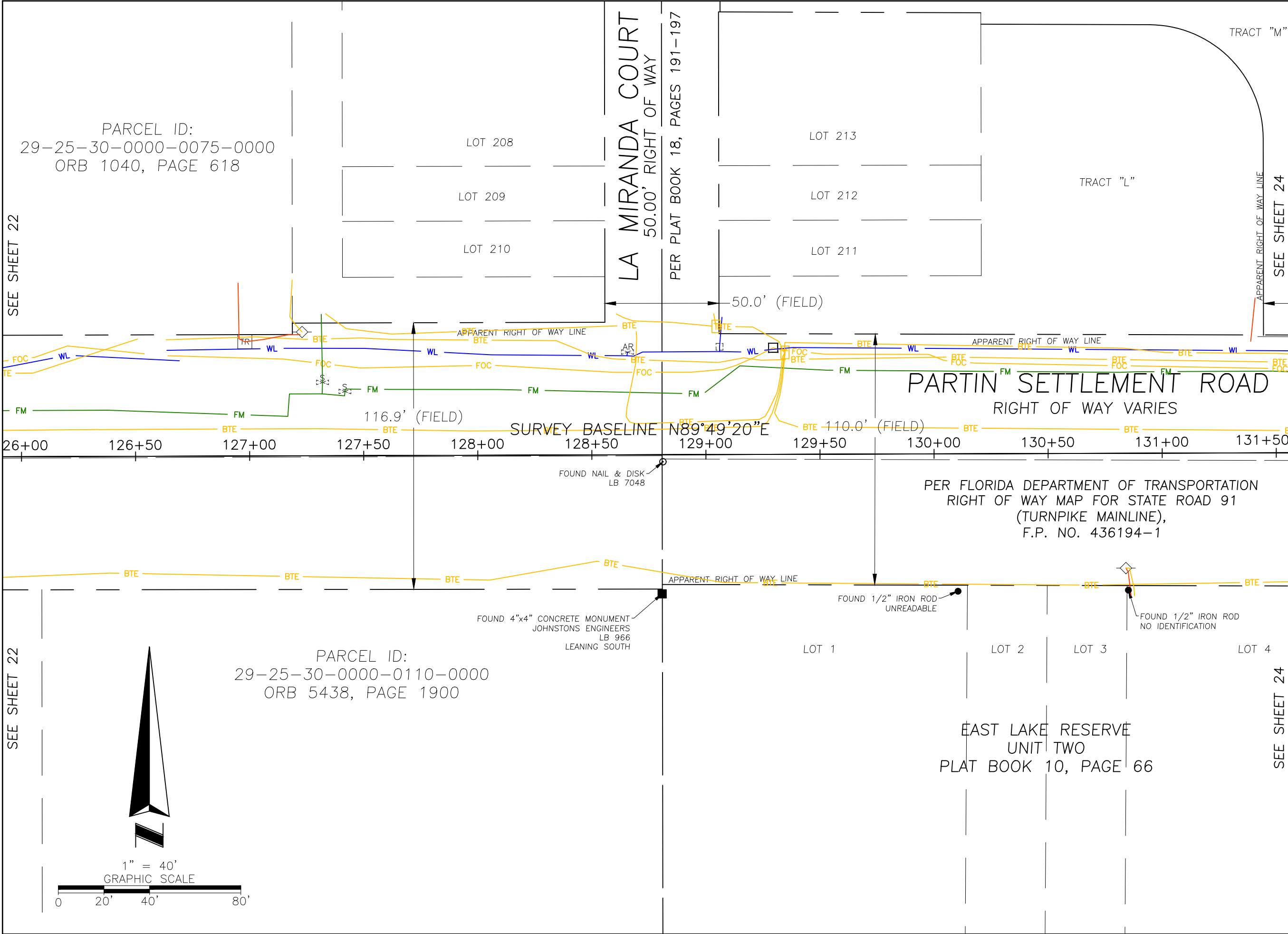
Project: Partin Settlement Road

Field Date: April 21, 2021

Drawn By: WMS

Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
22 OF 28



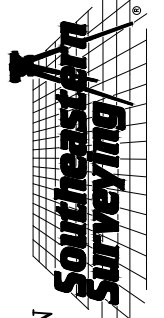
SEE SHEET 22

SEE SHEET 22

SEE SHEET 24

SEE SHEET 24

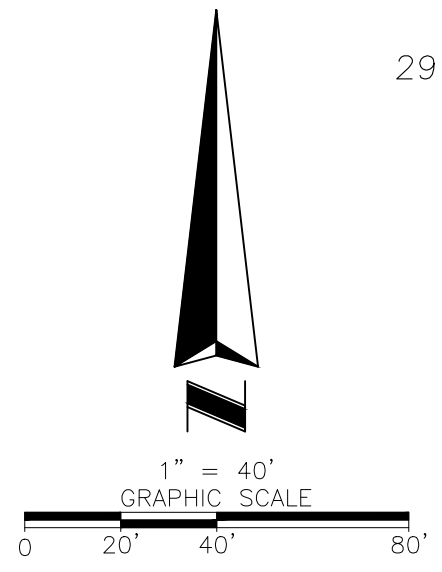
SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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Certification Number LB2108 e-mail:
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PRELIMINARY R/W EXHIBIT	
Project:	Partin Settlement Road
Field Date:	April 21, 2021
Drawn By:	WMS
Scale:	1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
23 OF 28



SEE SHEET 23

SEE SHEET 23

COSTA MESA LANE
RIGHT OF WAY VARIES

PER PLAT BOOK 18, PAGES 191-197

APPARENT RIGHT OF WAY LINE

TRACT "B"

LOT 1 LOT 2 LOT 3 LOT 4 LOT 4 LOT 5 LOT 6 LOT 7

LAGO BUENDIA PHASE 1
PLAT BOOK 18, PAGES 191-197

SEE SHEET 25

100.0' (FIELD)

TRACT "B"

APPARENT RIGHT OF WAY LINE

PARTIN SETTLEMENT ROAD
RIGHT OF WAY VARIES

SURVEY BASELINE N89°49'20"E

110.0' (FIELD)

132+00 132+50 133+00 133+50 134+00 134+50 135+00 135+50 136+00 136+50 137+00

PER FLORIDA DEPARTMENT OF TRANSPORTATION
RIGHT OF WAY MAP FOR STATE ROAD 91
(TURNPIKE MAINLINE),
F.P. NO. 436194-1

FOUND 4"x4" CONCRETE MONUMENT
JOHNSTONS ENGINEERS
LB 966

LOT 4

FOUND 1/2" IRON ROD
UNREADABLE

LOT 3

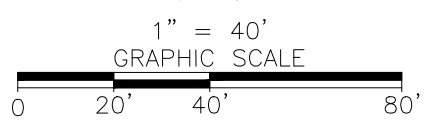
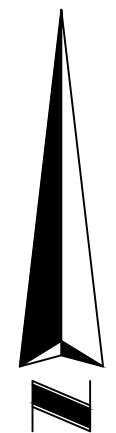
FOUND 1/2" IRON ROD
UNREADABLE

LOT 2

FOUND 1/2" IRON ROD
UNREADABLE

LOT 1

FOUND 4"x4" CONCRETE MONUMENT
JOHNSTONS ENGINEERS
LB 966

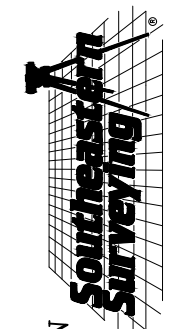


EAST LAKE RESERVE
PLAT BOOK 18, PAGE 65

PARCEL ID:
28-25-30-0000-0037-0000
ORB 4406, PAGE 1387

SEE SHEET 25

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



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info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

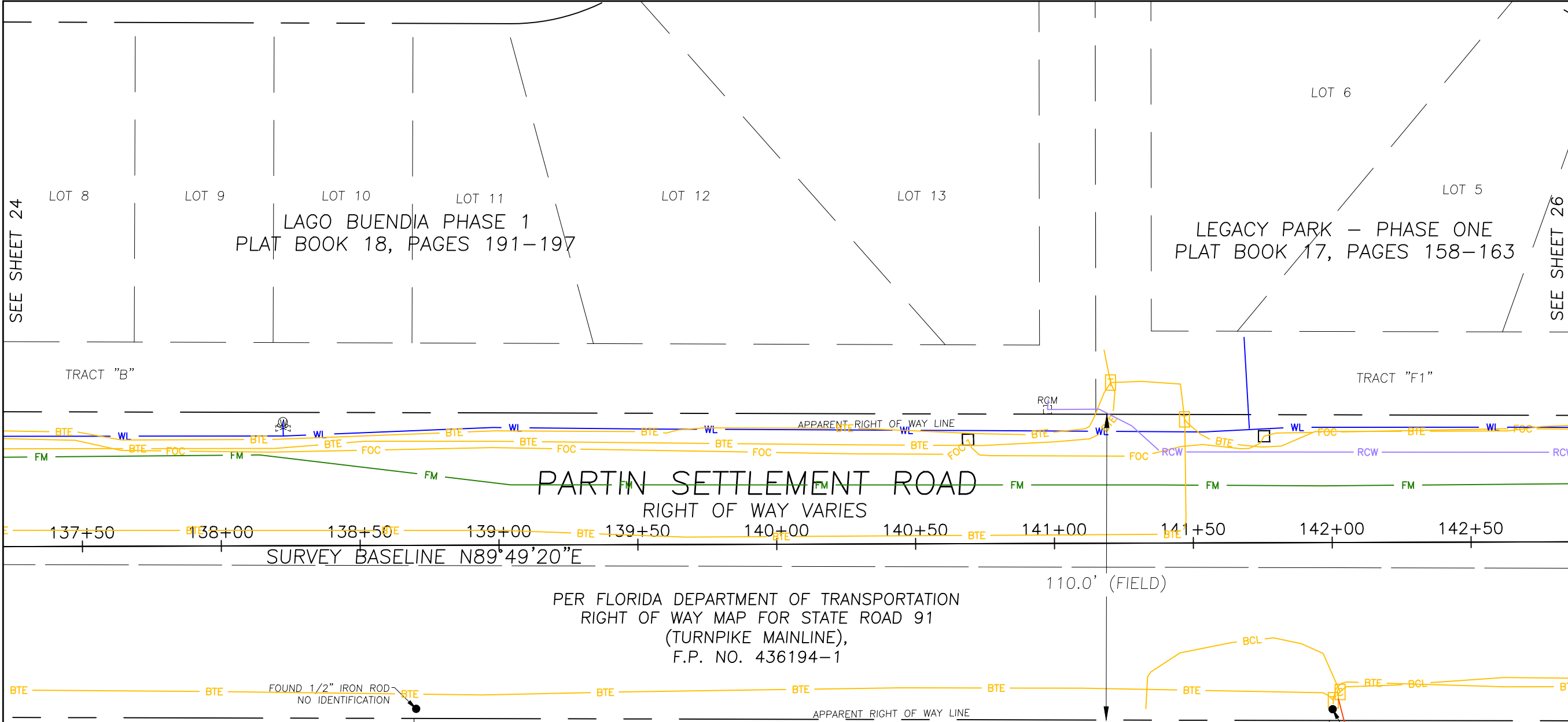
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Field Date: April 21, 2021

Drawn By: WMS

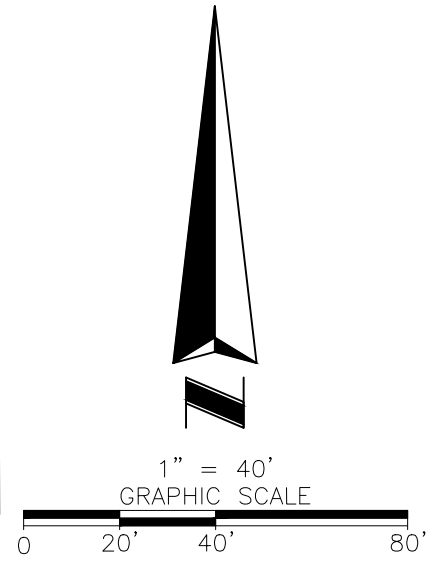
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
24 OF 28



SEE SHEET 24

PARCEL ID:
28-25-30-0000-0037-0000
ORB 4406, PAGE 1387

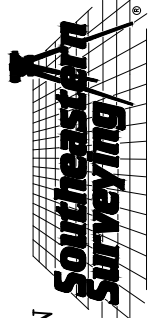


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28-25-30-0000-0032-0000
ORB 5099, PAGE 1569

PARCEL ID:
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ORB 5704,
PAGE 1673

SEE SHEET 26

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



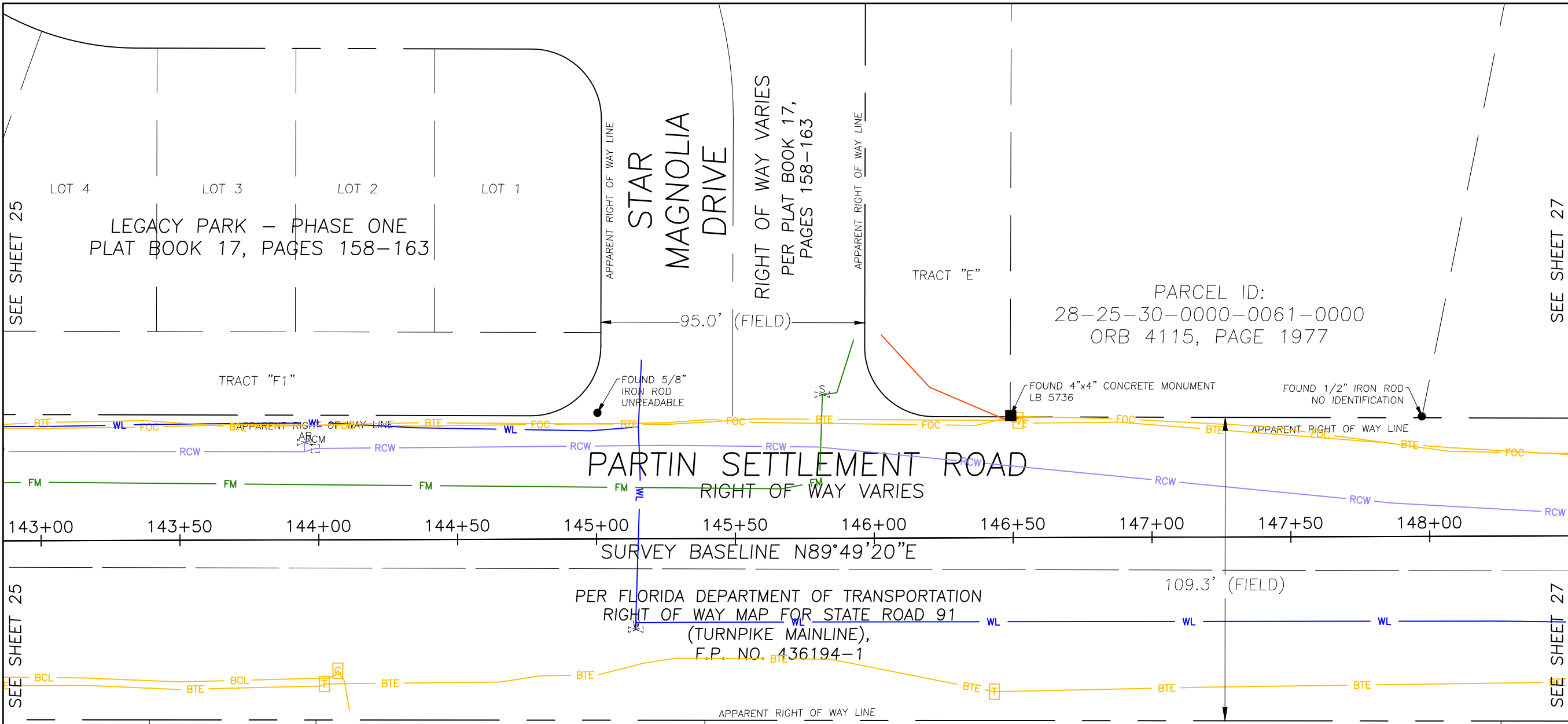
**SOUTHEASTERN SURVEYING
AND MAPPING CORPORATION**
6500 All American Boulevard Orlando,
Florida 32810-4350 (407) 292-8580
Certification Number LB2108 e-mail:
info@southeasternsurveying.com

PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
25 OF 28



SEE SHEET 25

PARCEL ID:
28-25-30-0000-0106-0000
ORB 5704,
PAGE 1673

PARCEL ID:
28-25-30-0000-0104-0000
ORB 5645,
PAGE 912

PARCEL ID:
28-25-30-0000-0107-0000
ORB 2222, PAGE 2676

PARCEL ID:
28-25-30-0000-0103-0000
ORB 2194, PAGE 2325

PARCEL ID:
28-25-30-0000-01000-0000
ORB 2095, PAGE 1704

APPARENT RIGHT OF WAY LINE

1" = 40'
GRAPHIC SCALE

0 20' 40' 80'

SEE SHEET 27

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.

Southeastern Surveying

SOUTHEASTERN SURVEYING
AND MAPPING CORPORATION

6500 All American Boulevard Orlando,
Florida 32810-4350 (407) 292-8580
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PRELIMINARY R/W EXHIBIT

Project: Partin Settlement Road

Field Date: April 21, 2021

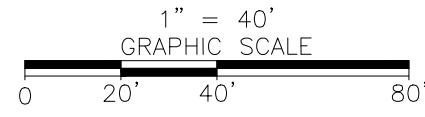
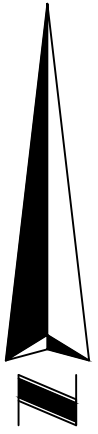
Drawn By: WMS

Scale: 1" = 40'

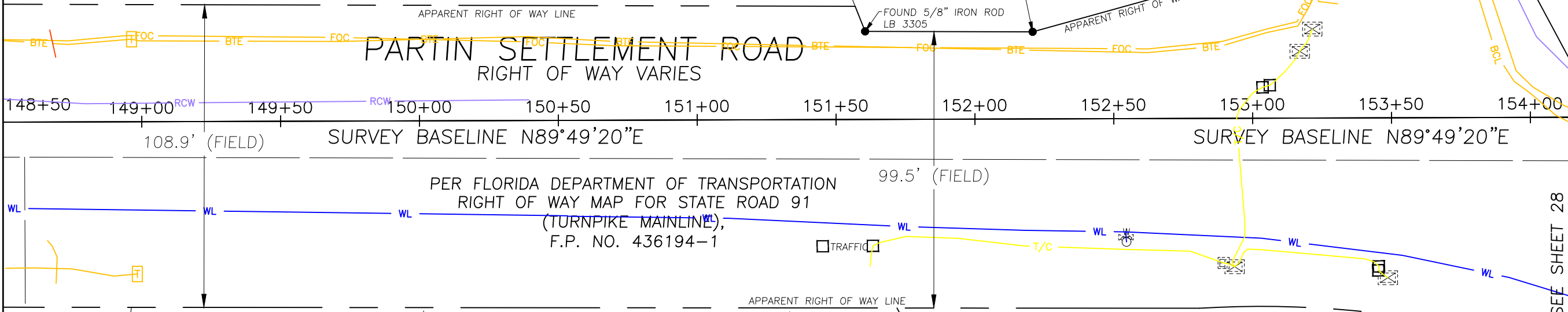
DRAWING NUMBER
64576 PRELIM RW

SHEET
NUMBER
26 OF 28

SEE SHEET 26



PARCEL ID:
28-25-30-0000-0061-0000
ORB 4200, PAGE 1019



SEE SHEET 26

PARCEL ID:
28-25-30-0000-01000-0000
ORB 2095, PAGE 1704

PARCEL ID:
28-25-30-0000-0105-0000
ORB 2194, PAGE 2333

PARCEL ID:
28-25-30-0000-0108-0000
ORB 2194, PAGE 2342

OCEAN GULF ESTATES, SECTION II
PLAT BOOK 2, PAGE 56

LOT 1

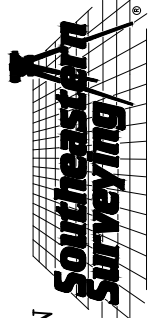
LOT 2

LAKESHORE
BOULEVARD EAST
PER PLAT BOOK 1, PAGE 359
60.00' RIGHT OF WAY

SEE SHEET 28

SEE SHEET 28

SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



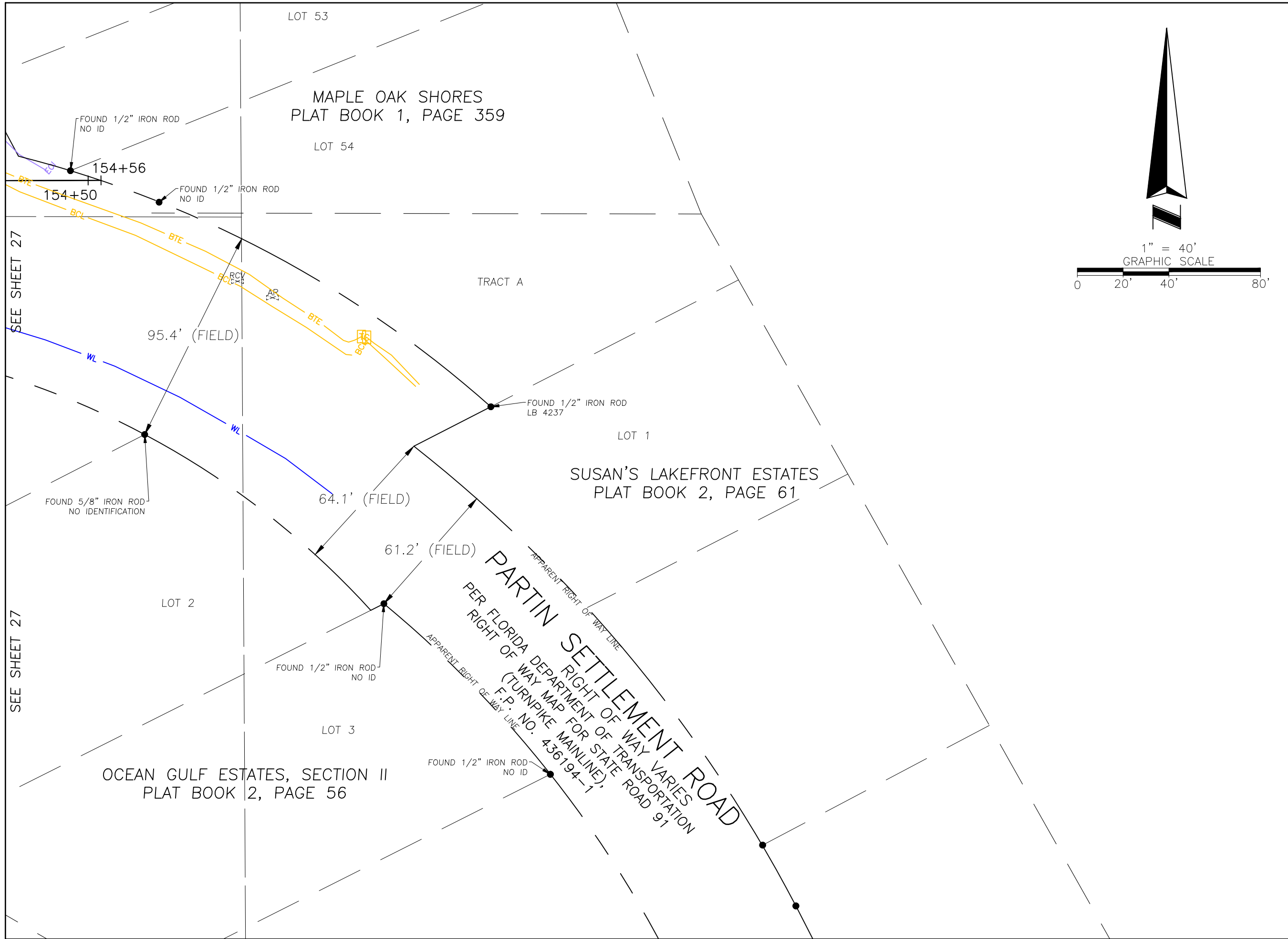
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PRELIMINARY R/W EXHIBIT

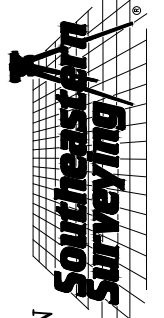
Project: Partin Settlement Road

Field Date: April 21, 2021
Drawn By: WMS
Scale: 1" = 40'

DRAWING NUMBER
64576 PRELIM RW
SHEET
NUMBER
27 OF 28



SEE SHEET 1 FOR NOTES,
LEGEND AND DESCRIPTION.



**SOUTHEASTERN SURVEYING
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PRELIMINARY R/W EXHIBIT	Scale: 1" = 40'
Project: Partin Settlement Road	Drawn By: WMS
Field Date: April 21, 2021	

DRAWING NUMBER
64576 PRELIM RW
SHEET NUMBER
28 OF 28



APPENDIX C: SUMMARY OF RESPONSES TO COMMENTS RECEIVED FROM THE PUBLIC

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

DATE	NAME	EMAIL / ADDRESS / PHONE	QUESTION / COMMENT	RESPONSE
<p>BELOW ARE ASSOCIATED WITH THE PUBLIC MEETING ON 1-21-21</p>				
<p>1/21/21 (Prior to Public Meeting) Already Transmitted</p>	<p>Daniel Bumpus</p>	<p align="center"> dan@bumpusandassociates.com 603 Front Street Celebration, FL 34747 407-566-0200 </p>	<p> From: Daniel Bumpus [mailto:dan@bumpusandassociates.com] Sent: Thursday, January 21, 2021 8:40 AM To: Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG> Cc: Walter Arias <pastorwalterarias@gmail.com>; Paulina Toro <ptoro214@gmail.com>; Daniel Bumpus <dan@bumpusandassociates.com> Subject: Martin settlement road </p> <p>Conroy, we will be attending the meeting tonight online, but I have the following questions that I would like to have addressed tonight if possible.</p> <ol style="list-style-type: none"> 1. Will the road design include right and left turn lanes to Simmons Road? 2. Has a decision been made on where additional ROW would be taken? 3. If ROW is taken from the Centro Cristiana Church and the retention pond and landscaping are impacted, will the county pay for restoring the pond and the landscaping? 4. Has a timeline for design and construction been established? <p>thanks danny bumpus Daniel L. Bumpus Bumpus and Associates, Inc 603 Front Street Celebration, FL 34747 407-566-0200</p>	<p> From: Carolyn Fitzwilliam <Carolyn.Fitzwilliam@qcausa.com> Sent: Tuesday, January 26, 2021 1:46 PM To: dan@bumpusandassociates.com Cc: Smith, Greg <GSmith@jmt.com>; Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG>; Shari Croteau <Shari.Croteau@qcausa.com>; Daniel Kelly <Daniel.Kelly@OSCEOLA.ORG>; Todd Hudson <todd.hudson@OSCEOLA.ORG>; Marianne Arneberg <Marianne.Arneberg@OSCEOLA.ORG>; Sally Myers <sally.myers@OSCEOLA.ORG> Subject: FW: Martin settlement road- Response to Mr. Bumpus </p> <p>Dear Mr. Bumpus, Thank you for your questions regarding the Partin Settlement Road improvements project. Please see responses to your questions below. Please note that this project is still in the alternatives' evaluation phase, and no final decisions have yet been made on the potential improvements, including a preferred typical section or alignment. The answers below reflect our preliminary concepts.</p> <ol style="list-style-type: none"> 1. Will the road design include right and left turn lanes to Simmons Road? The preliminary concept includes a left turn at Simmons Road. However, forecasted traffic demands do not indicate a need for a dedicated right turn lane at Simmons Road at this time. 2. Has a decision been made on where additional ROW would be taken? Right-of-way has not yet been determined. The County has developed several alternatives regarding different typical sections and alignments. After the public meeting on January 21, the project team will refine the potential alternatives, taking public comment into

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

				<p>consideration. Then County staff will recommend a preferred typical section and alignment to the Board of County Commissioners, which will vote to authorize advancement of the preferred alternative to design. The right of way needs will be determined during the design phase.</p> <p>3. If ROW is taken from the Centro Cristiana Church and the retention pond and landscaping are impacted, will the county pay for restoring the pond and the landscaping? As mentioned, right-of-way needs have not been determined. Once right-of-way needs are established, Osceola County would begin negotiations with the impacted property owners. Details such as this would be addressed at that time.</p> <p>4. Has a timeline for design and construction been established? The current project schedule shows design finishing in late 2022, with construction starting in 2023. Please note the schedule could change for a variety of reasons. Osceola County will continue to keep the community updated on the plans for the improvements.</p> <p>Please let us know if you have any additional questions.</p> <p>Thank you, Carolyn</p>
<p>1/21/21 (VPM)</p>	<p>Thomas Demmitt</p>	<p>tommacman21@gmail.com</p>	<p>Will a new (wider) overpass across the Florida Turnpike be needed?</p> <p>Will new bus routes for school children be developed with the construction changes/road closures?</p>	<p>Yes, this project is being coordinated with the Florida's Turnpike who intends to build two new bridges for Partin Settlement Road, one for each direction of traffic, as part of the Turnpike's mainline widening improvements.</p> <p>The goal of the traffic control plan is to minimize disruption of any current travel patterns. We do not anticipate any changes to school bus routes, but we are still in the study phase and have not begun final design.</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

<p>1/21/21 (VPM)</p>	<p>Wendy Fellner</p>	<p>wendi@wendiops.com</p>	<p>Are any changes planned to the intersection of Partin Settlement and Neptune to address congestion there?</p> <p>Is there any data available on injury rates for bicyclists using the on-road path vs separate multi-use sidewalk?</p>	<p>We are studying this area and have tentatively identified the option of providing dual southbound to eastbound right turn lanes as one means to improve congestion.</p> <p>At this point, we do not have any data available on injury rates for bicyclists using on-road paths versus a separate multi-use sidewalk.</p>
<p>1/21/21 (VPM)</p>	<p>John Leber</p>	<p>thelebers@msn.com</p>	<p>Our property is both on the Turnpike and Partin Settlement Road. Look like both you and the State are going to be taking land from Gateway Baptist Church. Is this a fair assessment of what you and the State are doing?</p>	<p>Right of way acquisition is anticipated for this project. Some is expected to be from the church, as well as along the south side of Partin Settlement Road from 1-92 to east of Red Jasper. We are trying to minimize those Right of way acquisitions as much as possible. Right of way needs will be finalized during design. For right of way questions, please contact Osceola County Right of W and Asset manager Sally Myers at 407-742-0502 or email at sally.myers@osceola.org.</p>
<p>1/21/21 (VPM)</p>	<p>Jason Lindsey</p>	<p>Jason.Lindsey@osceolaschools.net</p>	<p>Please schedule a meeting with the School District Planning Services Dept. regarding the access at the church entrance east of Remington Blvd. The School District has an access easement to use the church driveway for school use.</p>	<p>The County will contract the school district to discuss the planned improvements. Currently, the study intends to provide a left turn lane to allow traffic to enter the church driveway, though exiting traffic would only be allowed to make a right turn from the driveway.</p>
<p>1/21/21 (VPM)</p>	<p>Daisy Mendoza</p>	<p>daisymendoza@hotmail.com</p>	<p>Any plans to install a pedestrian bridge at U.S. 192?</p>	<p>Pedestrian safety is important and the County is always looking for opportunities to improve safety. The right-of-way is relatively tight in this area which limits the possibility of providing a pedestrian overpass, thus there are no plans at this time to construct a pedestrian bridge at U.S. 192. However, we will provide various pedestrian safety measures, such as signal phasing, as part of the improvements.</p>
<p>1/21/21 (VPM)</p>	<p>Jose Rodriguez</p>	<p>jrodriguez@lacasacristiana.org</p>	<p>Is there any plans on elevating the roadway between the existing bridge over the Turnpike and Remington intersection?</p>	<p>The new bridges over Florida's Turnpike will be at a higher elevation in order to span the proposed Turnpike widening which will require deeper girders and result in the need to raise the elevation of Partin Settlement Road above the current roadway.</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>Can we anticipate the building of turn lanes for businesses and churches at the medians?</p> <p>What can be expected with the drainage of the project of Phase 1?</p> <p>How can I know if the intersection at Remington will increase in linear length?</p> <p>Will the business and church properties get concrete driveways or asphalt with curb and gutter?</p> <p>Any light poles in Phase 1?</p> <p>Will the Turnpike project affect in any way the expected dates of construction and development?</p>	<p>We are still early in the design process, so we do not know just yet exactly where all the turn lanes will be located. the safety and operational goals of the project. Subject to achieving safety and operational goals for this project, the County will accommodate entrances to the businesses and churches along the corridor to the greatest extent possible.</p> <p>Currently, this area has an open ditch drainage system. The intent of this project is to provide a closed storm sewer system and route the drainage to ponds where the water can be treated and discharged in a gradual manner. The ponds help to improve water quality by separating out contaminants.</p> <p>Currently, there are no plans to change the present width of the north approach at the Partin Settlement Road/Remington Boulevard intersection.</p> <p>The type of driveways will depend on traffic demands and the functional use of property. The type of driveway will be determined during final design</p> <p>The County plans to include lighting as part of this project along the entire corridor.</p> <p>We are currently coordinating the construction of both projects. At this time, we do not expect the Florida's Turnpike project to affect this project.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>From: Pastor Jose Rodriguez <jrodriguez@lacasacristiana.org> Sent: Monday, March 1, 2021 11:25 AM To: Carolyn Fitzwilliam <Carolyn.Fitzwilliam@qcausa.com> Subject: Parton Settlement Road - Improvements Public Meeting Q&A</p> <p>Carolyn:</p> <p>I attended the Partin Settlement Road Improvements Public Meeting last January and had an active conversation during the Q&A. After the meeting we were told that we were going to receive definite answers to our questions in writing, are you able to provide us with any further information on this? Considering we haven't received the answers. It is in our best interest to understand the process and how it may affect us in the future.</p> <p>Thank you! Jose Rodríguez Casa Kids Pastor</p>	<p>From: Carolyn Fitzwilliam <Carolyn.Fitzwilliam@qcausa.com> Sent: Monday, March 1, 2021 11:32 AM To: Pastor Jose Rodriguez <jrodriguez@lacasacristiana.org> Cc: Shari Croteau <Shari.Croteau@qcausa.com> Subject: RE: Parton Settlement Road - Improvements Public Meeting Q&A</p> <p>Dear Pastor Rodriguez, Thank you for following up. We emailed the attached response on February 8. I apologize that you did not receive it.</p> <p>Please let us know if you have any additional questions.</p> <p>Thank you!</p> <p>(attached letter) From: Shari Croteau To: jrodriguez@lacasacristiana.org Subject: Partin Settlement Road Improvements Project Date: Monday, February 8, 2021 12:18:16 PM Attachments: image001.png</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>Jose Rodriguez jrodriguez@lacasacristiana.org Subject: Public Meeting – Response to Questions/Comments Partin Settlement Road Improvements From Neptune Road to Lakeshore Boulevard Osceola County, Florida Project No.: PS-20-11504-DG Dear Mr. Rodriguez, On behalf of Osceola County, I would like to thank you for attending the public information meeting for the Partin Settlement Road Improvements Project on Thursday, January 21, 2021, and for sharing your questions with us. You had several questions, including if there are any plans to elevate the roadway between the existing bridge over the Florida’s Turnpike and the Remington Boulevard intersection. The new bridges over Florida’s Turnpike will be at a higher elevation in order to span the proposed Turnpike widening, which will require deeper girders and result in the need to raise the elevation of Partin Settlement Road above the current roadway. You asked if the project anticipates building turn lanes for businesses and churches at the medians. The exact locations of median openings and turn lanes will be finalized during the design phase once the County has chosen a preferred alternative. Subject to achieving safety and operational goals for this project, the County will accommodate entrances to the businesses and churches along the corridor to the greatest extent possible. You also asked what can be expected with the drainage of the project in Phase One. Currently, this area has an open ditch drainage system. The intent of this project is to provide a closed storm sewer system and route the drainage to ponds. The ponds will help to clean the water and allow it to discharge slowly back into the ground. You wanted to know if the intersection at Remington Boulevard will increase in linear length. As shown in the public meeting displays, this project is not proposing to change the existing width of Remington Boulevard, and an appropriate median opening on</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>Is there a plan to take any property on the Sheriff property across from Gateway Baptist Church. We have already lost property in other project without compensation</p>	<p>Some right of way acquisition is expected along the south side of Partin Settlement Road from 192 to east of Red Jasper. Detailed right of way needs will be determined during the design phase.</p>
1/21/21 (VPM)	Karla Wong	karla.wong@osceola.org	<p>Noticed some of the alternatives had sidewalks that were different width. What determined which side of the road gets a wider/smaller sidewalk? Just curious</p>	<p>The four typical sections under consideration have sidewalks which range from 6 feet to 10 feet throughout most of the project. The sidewalk width will be determined by the availability of right of way.</p>
1/21/21 (VPM)	Daniel Bumpus	dan@bumpusandassociates.com	<p>Will phase one, which goes past Remington, also go to Simmons Road? Will improvements be made to the access to Simmons Road?</p> <p>If the taking of ROW involves modification of existing retention ponds, landscape and irrigation, will the county include the restoration in the construction?</p>	<p>This project may be divided into two parts, depending on traffic demands. The portion from Neptune Road to east of Remington will have transitions to existing Partin Settlement Road in the vicinity of Simmons Road, which will be further defined during final design.</p> <p>As mentioned, right of way needs have not been fully defined at this time. Once right of way needs are established, Osceola County will begin negotiations with the impacted property owners. Details such as this would be addressed at that time.</p>
1/21/21 (VPM)	Jeanie Donohue	jwu2you@gmail.com	<p>I am writing on behalf of Audubon & the Florida Sandhill Preservation Society. We have concerns regarding the 'Life Pathways' our resident endangered birds need to survive going forward. Also, given the experience of Melbourne who had high bird mortality foraging on their median strips, switched to paved or bricked strips w/no grassy swaths to lure & endanger our valuable eco-friendly avian birds.</p> <p>ALSO, increasing the traffic capabilities of this route around the lake significantly injures birds seeking to cross the road from East Lake into town.</p> <p>And if the answer that the construction of these improvements will not impact the avian or native</p>	<p>As part of the evaluation for each alternative, environmental assessments have been conducted, including surveys regarding impacts to endangered or protected species within the proposed improvement area. Our study revealed no adverse effects to endangered or protected species.</p> <p>The scope of this project is intended to end the Partin Settlement Road widening on the western approach to the Lakeshore Boulevard intersection. Consequently, the traffic capabilities of the existing roads around the lake would not be increased.</p> <p>While studies have been conducted regarding impacts to endangered or protected species within the proposed improvement area, there</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>creatures, have there actually been any studies done???</p> <p>In addition, I am concerned about the rates of traffic speed that could pick up on these new arterial routes. Slow walking birds & little old ladies (speaking from experience!), don't have much chance crossing fast moving roads as were improved at the corner of Route 15 & Rummell, making Rummell/Lakeshore a much more crowded road. There need to be many places made on the lake side of these road improvements to accomodate crossing of pedestrians, both human & animals. KInd of like bear & deer bridges farther North in Florida. Having come upon accidents on the two lane Partin Settlement Road, it would be good to be improved, but our groups do want to ensure that one of the great eco-draws to our area, our fantastic birds, are especially considered in the planning.</p> <p>Also, speaking up for the Native Plant Society & Audubon, we would very much like to be consulted on any plantings & avian-friendly landscaping to be planned around the retention ponds/wetland options along any of the planned improvements.</p> <p>The Sandhill Crane Preservation Society in particular feels that the construction of the median strips needs to be made with consultation with our organization, as if wrongly constructed with turf, etc. can be big draws for the Cranes, thus putting them in harm's way.</p> <p>Would like the Project Management to draw on the input of the Florida Sandhill Crane Preservation Society, President, Jeanie Donohue ~(407)738-7001, jwu2you@gmail.com. Our organization works with the FWC to protect these particular birds who are critically imperiled & endangered on the National Records. The land at</p>	<p>have been no studies conducted regarding the indicated issue at Lakeshore Boulevard.</p> <p>The proposed speed limits on Partin Settlement Road are expected to either remain the same or be reduced.</p> <p>The County is available to meet with interested members of the public to discuss this topic.</p> <p>We appreciate your interest and will be in touch to discuss your concerns and suggestions.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>the new eastern terminus plan of the Partin Settlement Road is the prime area for a full 1/10 of the entire population of these birds & needs to be considered carefully or our county will have extinguished a very great asset for both our citizens & international birders. Life pathways, signage, median construction & speed limits in affected area should be talked considered by our mutual conversations on each of these topics.</p> <p align="center">Thank-you, J. Donohue</p> <p>Also, of course, it would be great if your Project Management plan could include our local Kissimmee Valley Audubon Society's expertise in ensuring the planting of any project be beneficial to both humans & our valued avian population. Our local Pine Lily Chapter of the Native Plant Society could assist in ensuring the retention/wetland areas are in keeping with our Native species. Water run off & retention areas could be enhanced as well through the expertise of our local FL Institute of Science water quality forward thinking scientists. Respectfully, J. Donohue</p> <p>On a personal level, my home is on an important extension of these Partin Settlement improvements. For almost 3/4 of a century my home has been situated along this route. The traffic, both auto & laden trucks & equipment that has increased on this route since the improvements at the terminus of 15 & (Rummell/Lakeshore extention) resulting in an notable increase in the vibrations along this route, Will extra traffic along the traffic routed to Lakeshore Drive further increase the vibrations which are shaking the very foundations of my dwelling & surely others along this route?</p> <p>The allowable speeds of any vehicle, especially heavier ones, impacts the settling of the predominantly sandy soils in the areas. For this reason alone, I would lobby for keeping the speed limits below 35mph, ideally 30mph & 25mph in</p>	<p>We appreciate your interest and will be in touch to discuss your concerns and suggestions.</p> <p>The new roadway will be built to current standards to address the anticipated truck traffic.</p> <p>Traffic speeds are determined after engineering analyses have been undertaken to determine the prevailing operating speed of motorists. The proposed speed limits on Partin Settlement Road are expected to either remain the same or be reduced.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>areas where there are known animal, children & human habitation.</p> <p>I am speaking predominantly regarding the eastern extension of these Partin Settlement plans. If I, as a citizen with known friends who are bike riders, and considering that young people on skateboards & bicycles would be in harm's way if any of your plans allowed for only "experienced" bike riders. So, a wider pedestrian option sounds good:)</p> <p>But, really, would like some serious consultation with your project management with our Sandhill Crane experts. We have already been given the green light by the Osceola County Planning Director to begin work on signage for the Sandhill Cranes in particular. (Signage to be place throughout Osceola County at intersections where the Cranes need to cross existing roadways.) ...As East Lake proximal to the eastern extension of your proposed improvements is where our FWC Imperiled Cranes both roost in the evenings, as well as brood & rear their chicks, this area is extremely important to plan with them in mind. Eco-tourism/Birding is second only to boating as a recreational draw for tourists & an asset to our citizenry, so any &/all efforts to accommodate our avian population would obviously benefit the birds, but our County's income as well.</p> <p>Will reach out via other methods you suggested, Thomas, on behalf of the Sandhills & our local Citizen Scientists at the Kissimmee Valley Audubon Society. We appreciate the opportunity to have the County Planners consider the input of their citizens & local conservation organizations.</p> <p>Sincerely, Ms. Jeanie Donohue</p>	<p>Your preference for wider pedestrian sidewalks rather than on-road bicycle lanes will be considered during the evaluation of alternatives.</p> <p>We appreciate your interest and will be in touch to discuss your concerns and suggestions.</p>
<p>1/21/21 (In Person Public Meeting)</p>	<p>April Bennett</p>	<p>Aprilyn729@hotmail.com</p> <p>1100 E. Lakeshore Blvd. Kissimmee, FL 34744</p>	<p>House on the corner has a drain filled at the property line. Please no draining at this property.</p> <p>1) Option 1A 2) Option 3</p>	<p>Currently, this area has an open ditch drainage system. The intent of this project is to build enclosed stormsewer system which will take the drainage to ponds where the water could be treated and then discharged in a gradual</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

		<p align="center">321-624-4654</p>	<p>From: April Bennett <aprilyn729@hotmail.com> Sent: Monday, February 8, 2021 1:00 PM To: Shari Croteau <Shari.Croteau@qcausa.com> Subject: Re: Partin Settlement Road Improvements Project</p> <p>Thank you for that information however my concern was people driving in my yard since my drain field is on the edge of the easement. I do not vehicles of any type driving through my yard. It has happened in the past with surveying and cabling. Drainage was not the concern it is my drain field for my septic. I was told that this would be noted.</p> <p>April Bennett</p> <p>From: April Bennett <aprilyn729@hotmail.com> Sent: Tuesday, February 9, 2021 5:52 PM To: Shari Croteau <Shari.Croteau@qcausa.com> Subject: Re: Partin Settlement Road Improvements Project</p> <p>Ok I feel completely misunderstood at this point. It is not people leaving the roadway it is construction</p>	<p>manner back into the existing ditch system along the corridor.</p> <p>From: Shari Croteau <Shari.Croteau@qcausa.com> Sent: Tuesday, February 9, 2021 1:24 PM To: April Bennett <aprilyn729@hotmail.com> Subject: RE: Partin Settlement Road Improvements Project</p> <p>Ms. Bennett,</p> <p>I apologize, I misunderstood your question. The Partin Settlement Road Improvements will have curb and gutter throughout the corridor along with a closed drainage system. We anticipate that this feature may be able to minimize vehicles from leaving the roadway near your property.</p> <p>If you have any other questions or concerns, please do not hesitate to contact project manager Conroy Jacobs at 407-742-0557 or by email, Conroy.Jacobs@osceola.org.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>vehicles, your surveyors and other equipment used for the construction. My yard always ends up a parking spot when anything is being done around here. You do not have to respond at this point I feel like I am NOT being heard!</p> <p>April Bennett</p>	<p>From: Shari Croteau <Shari.Croteau@qcausa.com> Sent: Wednesday, February 10, 2021 11:29 AM To: April Bennett <aprilyn729@hotmail.com> Subject: RE: Partin Settlement Road Improvements Project</p> <p>Ms. Bennett,</p> <p>Again, apologies. I see you live at 1100 E. Lakeshore Blvd., Kissimmee. I wish to assure you that construction and survey vehicles are not permitted to store equipment or park vehicles on private property without the property owner's consent. They are only allowed to park in the public right of way. We will request that the County advise their contractors and surveyors to be aware of your concerns.</p> <p>I can also have a member of the project team contact you by phone to discuss this more in detail if you like. Otherwise, please do not hesitate to contact me if you have any other questions or concerns.</p> <p>Thank you.</p>
1/21/21 (In Person Public Meeting)	Pastor Walter Arias	<p>ministeriodiosdepactos@gmail.com</p> <p>pastorwalterarias@gmail.com</p> <p>1130 Simmons Rd. Kissimmee, FL 34744</p> <p>407-668-2002 407-870-5858</p>	<p>I would like to be contacted in order to show our church plans and the need to have the county involve in a turn lane we are being requested to pay from our pocket. We strongly believe the city shall pay for this turn lane and we gladly will work with the city for the additional right of way the city needs for the proposed project. Please include our church and community plans.</p>	<p>The County will be in touch regarding this matter.</p>
1/28/21 (emailed after public meeting)	Michael Bast	<p>bastmichael8@gmail.com</p> <p>1720 Rachel Lane Kissimmee, FL 34744</p>	<p>After looking at the plans I am of the opinion that option 1a, Hold North R/W would be the best and least expensive way to achieve the goal. The existing path on Neptune is 8 feet wide and works</p>	<p>Your preference will be considered during the evaluation of alternatives.</p>


Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			well. If reduced the width of the new plan you save some expense and still achieve the goal.	
1/28/21 (called and left message after meeting)	Benti Miller	2070 Partin Settlement Road Kissimmee, FL 34744 407-288-2433	Ms. Benti Miller called and she would like to speak with a project team member. Her number is 407-288-2433.	
1/29/21 (emailed after public meeting)	Keith Smith	keith@icefactory.com 2221 Partin Settlement Rd Kissimmee, FL 34744	<p>From: Keith Smith [mailto:keith@icefactory.com] Sent: Friday, January 29, 2021 2:09 PM To: Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG> Subject: Project # PS-20-11504-DG Public Comment</p> <p>Hi Mr Jacobs,</p> <p>I'd like to pass along some comments and recommendations for the Partin Settlement Rd widening project.</p> <p>I'm the General Manager of the Ice Factory, located at 2221 Partin Settlement Rd.</p> <p>Based on the renderings that were available at the public meeting, I have a few concerns and questions.</p> <p>I'd like to recommend a left-hand turn lane into our property. I would venture to say that approximately 60-70% (if not more) arrive from the Shady Lane/Neptune road direction. Not having a left turn option into our property would likely cause more congestion and accidents as people attempt to U turn to come back to our property. Traffic comes in large inflows/outflows at our events. Having to make a U turn to come to our property could cause serious traffic concerns.</p> <p>I'd also like to recommend a right-hand turn into our facility for the same reason. To mitigate potential for rear end collisions, it would be ideal.</p>	<p>(Suggested responses)</p> <p>We are just starting the design process, so we do not know just yet exactly where all the turn lanes will be located. Subject to achieving safety and operational goals for this project, the County will accommodate entrances to the businesses and churches along the corridor to the greatest extent possible.</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>I also have construction concerns that I was able to mention to Mrs. Myers. We have concern about water and utility to our property. During the construction of Shady Lane, the contractor broke a water main causing significant business interruption to our operation and extensive additional costs to recover our ice sheet. Our refrigeration plant relies on water-based cooling. When we don't have water, we must turn off our refrigeration plant, and that could jeopardize our ice surface without refrigeration. I'd like to recommend that the county provide us with a standby water tank with pump that we could plumb into our refrigeration water supply to provide water for a duration of an outage. This would prevent the harm that a water line break would cause to our business. If a water outage is not corrected timely, the costs of the failure could in the high 10's of thousands in dollars to recover our ice sheet.</p> <p>While our refrigeration system has multiple failsafe features to prevent damage to the system and venting of ammonia (the refrigerant used in our system). A loss of water would cause a high-pressure situation that could cause a safety valve to release ammonia, which would result in a hazmat situation and subsequent evacuation of nearby apartments and businesses.</p> <p>I at the public meeting mentioned concerns about signage during heavy road construction so our customers can locate our facility, should they attempt to detour or be deterred by the construction to visit us. I'd like to ask the county to provide additional signage from alternate entry routes to reach our facility, at Aeronautical and Shady lane. The construction on Shady lane has also caused some issues with guests coming to our events.</p> <p>I'd also like to better understand the expected impact the construction may have on the</p>	<p>The County will be in touch regarding this matter.</p> <p>The County will be in touch regarding this matter.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>entryway to our property, and alterations that may be required to our sign, and entryway to provide ample safe access to the property by our customers.</p> <p>Thank you for your attention to this matter.</p> <p>Sincerely,</p>  <p>General Manager Ice Factory of Central Florida</p>	<p>The County will be in touch regarding this matter.</p>
<p>2/6/21</p>	<p>Ramon Fonseca</p>	<p>Ramonf66@earthlink.net 2998 Partin Settlement Rd.</p>	<p>From: Ramon Fonseca [mailto:ramonf66@earthlink.net] Sent: Saturday, February 6, 2021 5:49 PM To: Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG> Subject: Partin Settlement Road project</p> <p>Mr Jacobs,</p> <p>Hi, my name is Ramon Fonseca, I live at 2998 Partin Settlement Road, I heard about the project through a friend that lives in Lakeshore Blvd. I'm concerned because I have not received any notice about the public meetings discussing this project and I'm one of the potentially affected owners. If the road will be widened does that mean I will be loosing footage in my property, I have a fence along Partin Settlement, I also have 3 gates to my property, 2 vehicular and one personal, I also have running in my property storm water ditches. How will this affect me? Is there a way I can get notifications of meetings about this project?</p> <p>Thank you. Ramón Fonseca</p>	<p>Mr. Fonseca,</p> <p>I am sorry you did not receive the mail. We have checked our mailing list and ensured that your name and address is on there for future correspondence.</p> <p>Some right of way acquisition is expected along the south side of Partin Settlement Road from 192 to east of Red Jasper. The County has taken steps to minimize the right of way impacts where possible. Right of way will be refined during the design phase once the preferred alternative has been approved by the Board of County Commissioners. In the meantime, please contact Osceola County Right of Way and Asset Manager Sally Myers with any questions at 407-742-0502 or sally.myers@osceola.org.</p> <p>All the information regarding this project, including the presentation from the meeting, can be found online at www.osceola.org/go/partinsettlement. If you have any questions or comments, you can also contact project manager Conroy Jacobs at 407-742-0557 or at Conroy.Jacobs@osceola.org.</p>

Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

			<p>From: Shari Croteau <Shari.Croteau@qcausa.com> Sent: Monday, February 8, 2021 2:31 PM To: ramonf66@earthlink.net Subject: Re: Partin Settlement Road project</p> <p>Mr. Fonseca,</p> <p>Thank you for your email regarding the Partin Settlement Road Improvements Project. We apologize that you did not receive the notice regarding this project and have checked our mailing list to ensure that your name and address is correct for any future correspondence. We have placed all information from the meeting on the project web page at www.osceola.org/partinsettlement. Additionally, a member of our project team will be happy to discuss the project with you.</p> <p>Some right of way acquisition may be needed along the south side of Partin Settlement Road in the vicinity of your property at 2998 Partin Settlement Road, though the exact right of way needs will be determined during the design phase once the preferred alternative has been approved by the Board of County Commissioners. In the meantime, please contact Osceola County Right of Way and Asset Manager Sally Myers with any questions about the County's right of way process at 407-742-0502 or sally.myers@osceola.org.</p> <p>Please let us know if you would like to schedule a time to discuss the project. If you have any questions or comments, you may also contact project manager Conroy Jacobs at 407-742-0557 or at Conroy.Jacobs@osceola.org.</p> <p>Thank you.</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

<p align="center">2/26/21</p>	<p align="center">David Glancy</p>	<p align="center">Davidg716@gmail.com</p>	<p>From: David G [mailto:davidg716@gmail.com] Sent: Friday, February 26, 2021 3:18 PM To: Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG> Subject: Concerns about the Partin Settlement Road project</p> <p>Mr Jacobs,</p> <p>What is the plan for parent pickup with the new partin settlement road design? It looks like there will be no more left turns out of partin settlement elementary (which shares the driveway with Northeast Chrisitan Church) and the turn lane into the school from partin settlement (again, via the driveway for Northeast Chrstian Church) looks much shorter than what is there now. You will have a lot of cars blocking traffic in the left hand east bound lane between 2 and 3pm on a daily basis. How is this being handled?</p> <p>Also, I live in one of the houses that are right on partin settlement, are we going to have to go down to Simmons and make a u-turn now to head towards lakeshore? I drive this on a daily basis and will not appreciate having to do that every single day. Is Osceola country planning to compensate us for the loss in home value due to this?</p> <p>Thank You David Glancy</p>	<p>From: Shari Croteau <Shari.Croteau@qcausa.com> Sent: Tuesday, March 2, 2021 1:58 PM To: davidg716@gmail.com Cc: Conroy D Jacobs <Conroy.Jacobs@OSCEOLA.ORG>; Todd Hudson <todd.hudson@OSCEOLA.ORG>; Marianne Arneberg <Marianne.Arneberg@OSCEOLA.ORG>; Daniel Kelly <Daniel.Kelly@OSCEOLA.ORG>; Ray, Jennifer <JRay@jmt.com>; Flick, Jason <JFlick@jmt.com>; Smith, Greg <GSmith@jmt.com>; Carolyn Fitzwilliam <Carolyn.Fitzwilliam@qcausa.com> Subject: Re: Concerns about the Partin Settlement Road project</p> <p>Mr. Glancy,</p> <p>Thank you for your email regarding the Partin Settlement Road Improvements Project and sharing your concerns about traffic at the Partin Settlement Elementary School as well as your question about left turn movements from your property.</p> <p>Currently, the preferred alternative provides a left turn lane to allow eastbound traffic to enter the church driveway, though exiting traffic would only be allowed to make a right turn from the driveway onto Partin Settlement Road. We will be coordinating with the school during the design phase to ensure the project provides suitable accommodations for school traffic while also providing safe movements for students, parents and others traveling along Partin Settlement Road. We will also re-examine the length of the proposed left turn lane to provide suitable capacity for this movement.</p> <p>Similarly, the preferred alternative also provides a median in front of your property that would</p>
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Partin Settlement Road Widening from Neptune Road to Lakeshore Boulevard Alternatives Evaluation: PS-20-11504-DG

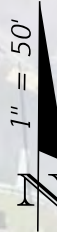
			<p>From: David G <davidg716@gmail.com> Sent: Tuesday, March 2, 2021 2:18 PM To: Shari Croteau <Shari.Croteau@qcausa.com>; Conroy.Jacobs@osceola.org Subject: Re: Concerns about the Partin Settlement Road project</p> <p>Thank you for your reply.</p> <p>I am having trouble understanding why this project is even needed? Outside the hours of 8a-9a and 2p-3p during school days there is very little traffic that uses Partin Settlement east past Remington. It seem like this entire project is a waste of resources and nothing more than an inconvenience to the property owners who have direct driveways onto partin settlement.</p>	<p>restrict left turns onto eastbound Partin Settlement Road. We are making improvements at the intersections to accommodate the U-turns safely and efficiently. The median is being proposed to enhance safety along the roadway by reducing the potential for head-on and left-turn crashes. Turning right and then making a U-turn can be more effective and safer than trying to turn left across several lanes of traffic. However, the exact location of median openings will also be determined during the design phase.</p> <p>The community will have an opportunity to review the design plans at another public meeting later this year and provide comments. In the meantime, we invite you to stay up to date on the project by visiting www.osceola.org/partinsettlement. If you have any questions or comments, you may also contact Osceola County Project Manager Conroy Jacobs at 407-742-0557 or at Conroy.Jacobs@osceola.org.</p> <p>Thank you.</p>



APPENDIX D: PREFERRED ALTERNATIVE PARTIN SETTLEMENT ROAD WIDENING PLANS

TRACT SW
PARTIN VILLAGE
PLAT BOOK 22, PAGES 23-24

TRACT C
PARTIN VILLAGE
PLAT BOOK 22,
PAGES 23-24



EXIST. R/W

CL PARTIN SETTLEMENT RD.

EXIST. R/W

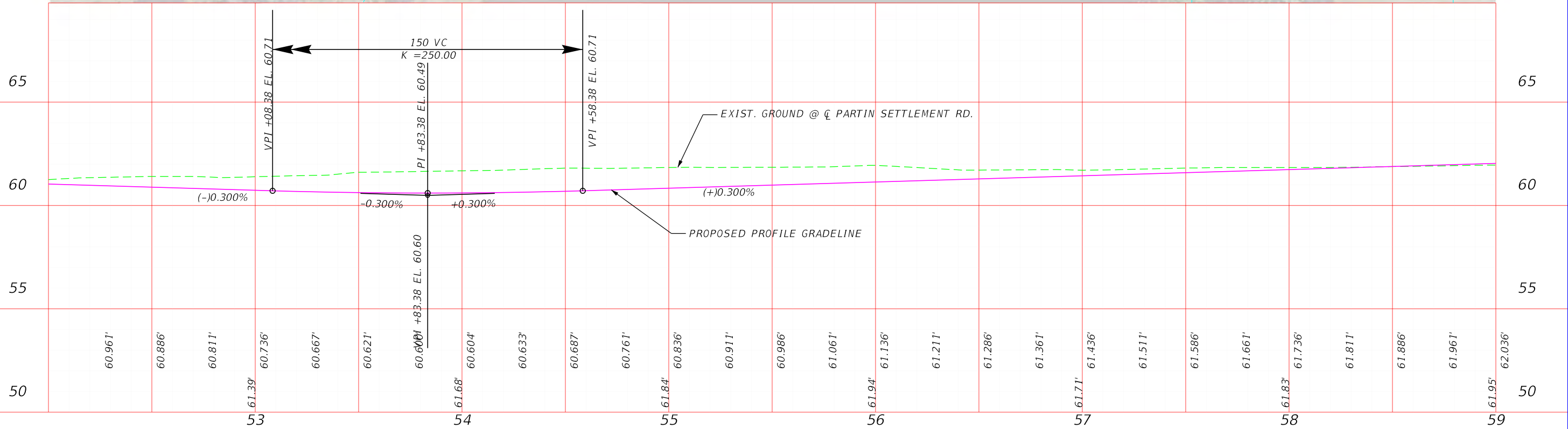
TRACT B
FISH LAKE - PHASE 1
PLAT BOOK 12, PAGES 151-152

EXIST. GROUND @ CL PARTIN SETTLEMENT RD.

PROPOSED PROFILE GRADELINE

MATCH LINE STA. 52+00.00

MATCH LINE STA. 59+00.00

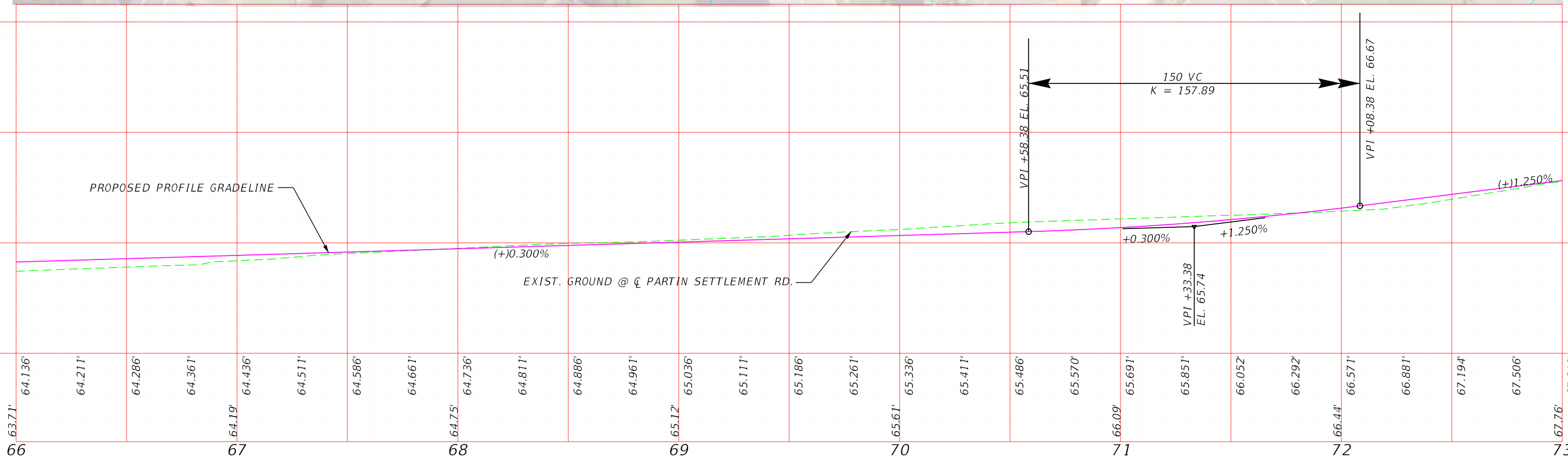
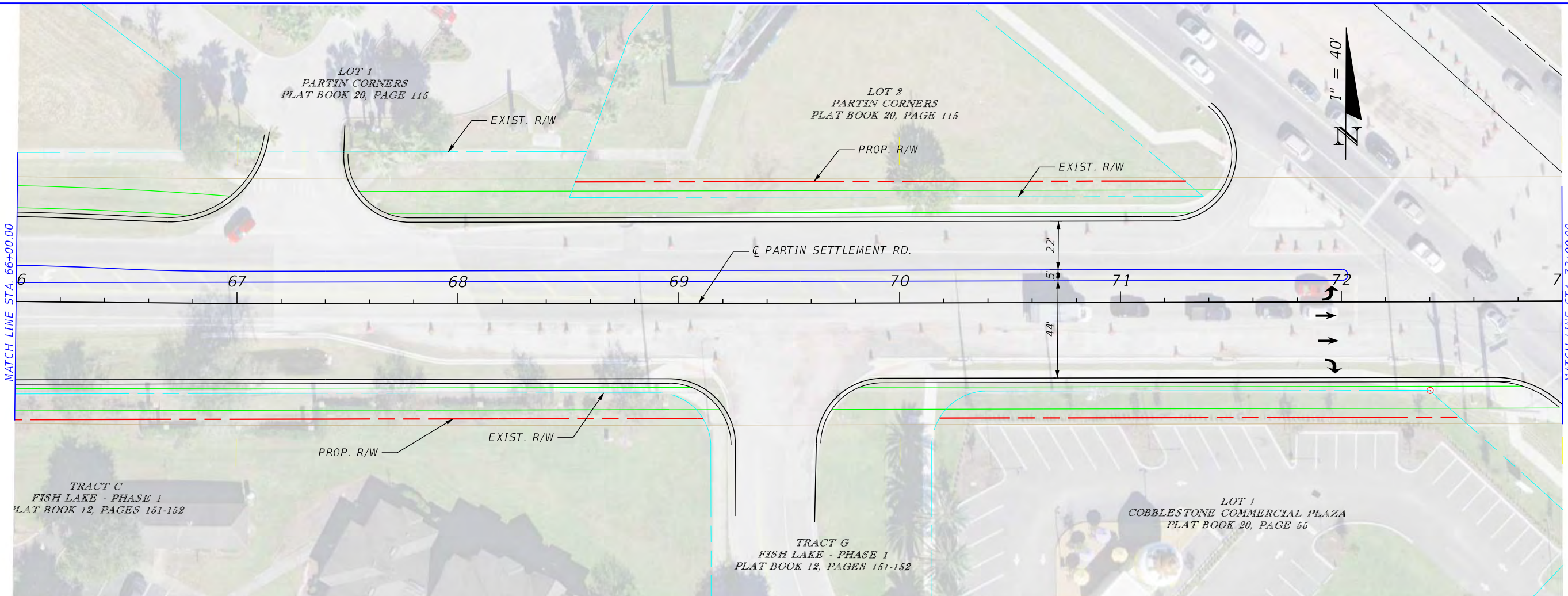


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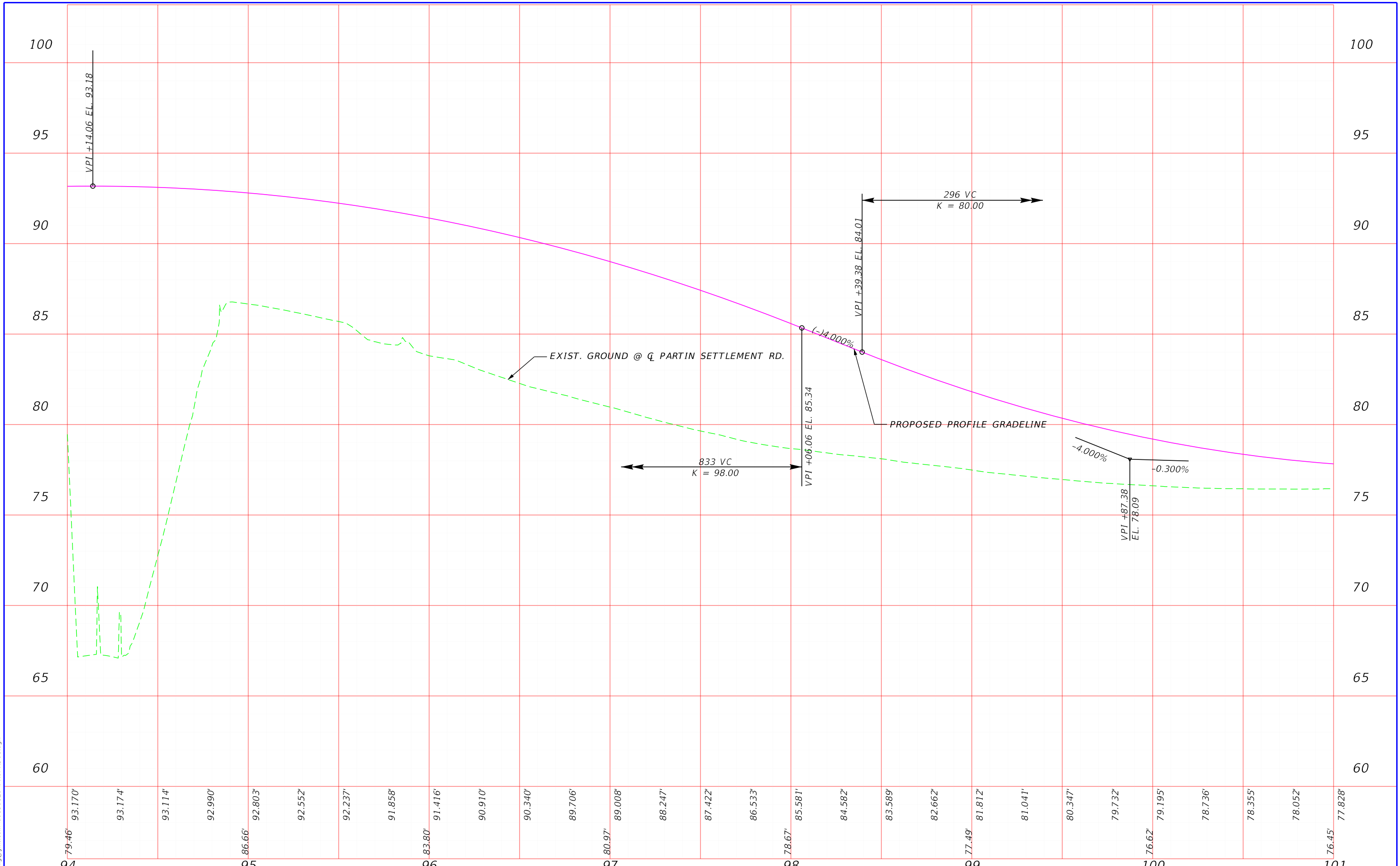
<p>PARTIN SETTLEMENT RD PLAN-PROFILE</p>
SHEET NO. 7



DATE		DESCRIPTION		DATE		DESCRIPTION	

OSCEOLA COUNTY PUBLIC WORKS DEPARTMENT			PARTIN SETTLEMENT RD PLAN-PROFILE	SHEET NO. 9
ROAD NO.	COUNTY	FINANCIAL PROJECT ID		

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DATE	DESCRIPTION	DATE	DESCRIPTION	ROAD NO.		COUNTY	FINANCIAL PROJECT ID

SOMERSET
PLAT BOOK 16, PAGES 178-179

TRACT 'D'
REMINGTON - PHASE 2



REMINGTON
BOULEVARD

EXIST. R/W

CL PARTIN SETTLEMENT RD.

22'

5'

44'

EXIST. R/W

PROP. R/W

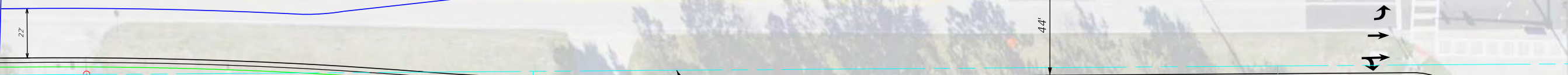
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-0165-0000
ORB 5223, PAGE 17

PARCEL ID:
29-25-30-0000-0160-0000
ORB 5301, PAGE 237

MATCH LINE STA. 101+00.00

MATCH LINE STA. 108+00.00

101 102 103 104 105 106 107 108



PARCEL ID:
29-0000-0170-0000
-0223, PAGE 17

PARCEL ID:

85 85

296 VC
K = 80.00

VPI +35.38 EL. 77.64

80 80

75 75

PROPOSED PROFILE GRADELINE

EXIST. GROUND @ CL PARTIN SETTLEMENT RD.

(-)-0.300%

70 70

77.681' 77.599' 77.524' 77.449' 77.374' 77.299' 77.224' 77.149' 77.074' 76.999' 76.924' 76.849' 76.774' 76.699' 76.624' 76.549' 76.474' 76.399' 76.324' 76.249' 76.174' 76.099' 76.024' 75.949' 75.874' 75.799' 75.724' 75.649'

102

103

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DATE	DESCRIPTION	DATE	DESCRIPTION

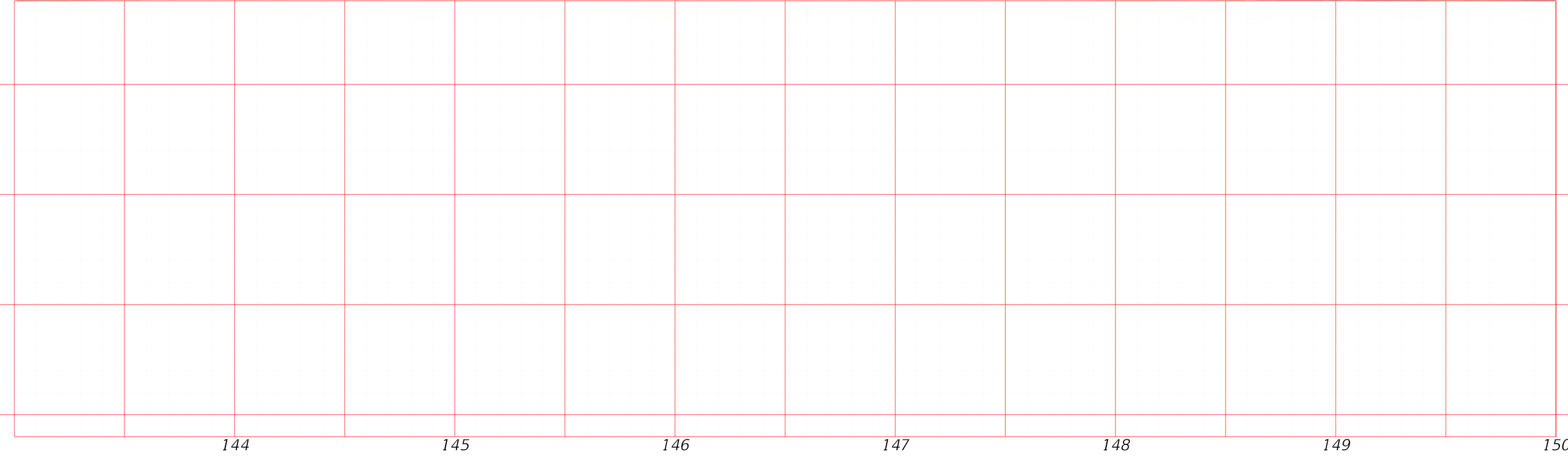
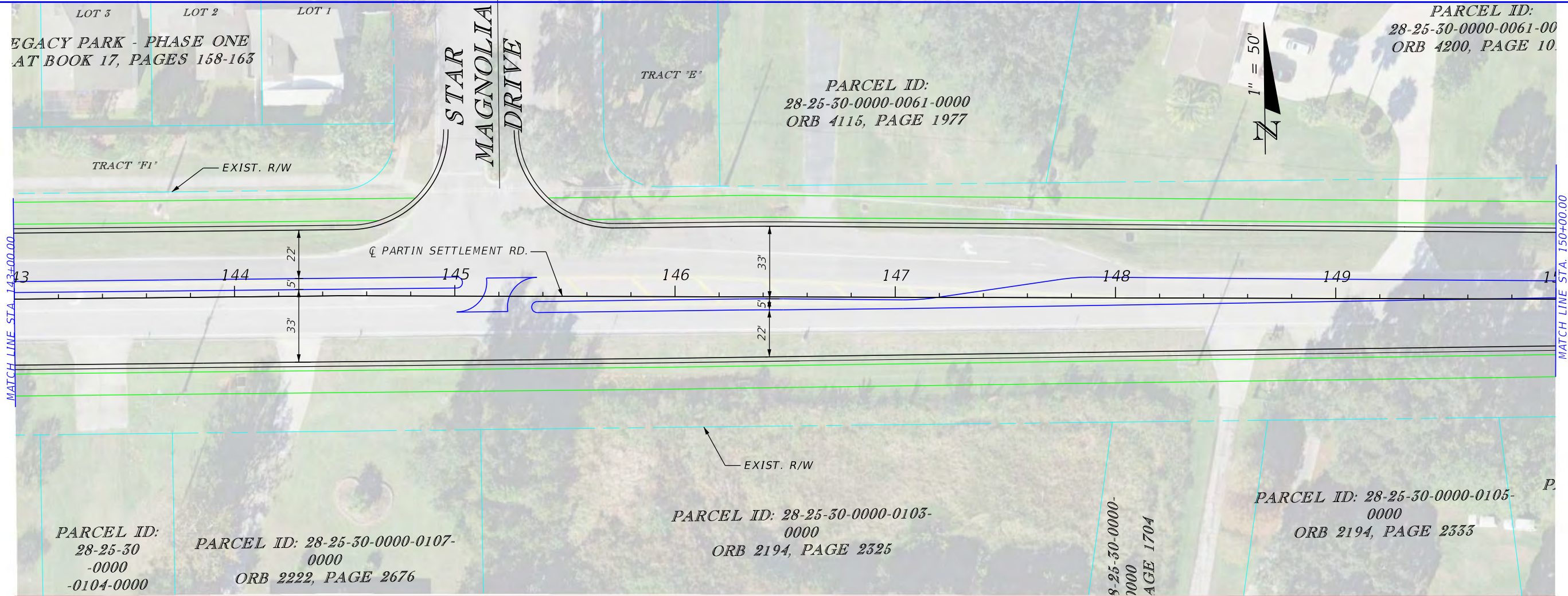
OSCEOLA COUNTY
PUBLIC WORKS DEPARTMENT

ROAD NO. COUNTY FINANCIAL PROJECT ID

PARTIN SETTLEMENT RD
PLAN-PROFILE

SHEET
NO.

15



REVISIONS				OSCEOLA COUNTY PUBLIC WORKS DEPARTMENT			PARTIN SETTLEMENT RD PLAN-PROFILE	SHEET NO. 21
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